



AGENDA
CHARTER TOWNSHIP OF MERIDIAN
MERIDIAN TRANSPORTATION COMMISSION
January 21, 2021 7:00 p.m.

1. CALL MEETING TO ORDER
2. APPROVAL OF AGENDA
3. APPROVAL OF MINUTES
 - A. November 19, 2020

<p>Zoom Meeting ID: 880 2838 5000 Password: 5000</p>

4. COMMUNICATIONS
5. PUBLIC REMARKS
6. ELECTION OF CHAIR AND VICE-CHAIR
7. COMMISSION DISCUSSION
 - A. 2021 Local Road Program
 - B. Pathway Projects
 - C. Okemos Road-Downtown
8. NEXT MEETING DATES
 - A. Township Board Meeting January 26, 2021 (Boards/Commissions)
 - B. Transportation Commission Meeting March 18, 2021.
9. ADJOURNMENT

Individuals with disabilities requiring auxiliary aids or services should contact Director of Community Planning and Development Mark Kieselbach, 5151 Marsh Road, Okemos, MI 48864 or 517.853.4506 - Ten Day Notice is Required.
Meeting Location: 5000 Okemos Road, Okemos, MI 48864 Central Fire St.

Providing a safe, welcoming, sustainable, prime community.





Charter Township of Meridian
Meridian Transportation Commission
5151 Marsh Road, Okemos, MI 48864
Thursday, November 19, 2020– Minutes -DRAFT

Members

Present: Chair Vagnozzi, Vice-Chair Lovell, Commissioners Potter, Deschaine, Hudson and Robertson

Members

Absent: Commissioner Kolbasa

Others

Present: Bill McConnell, Police Chief Ken Plaga and Community Planning & Development Director Mark Kieselbach

1. CALL MEETING TO ORDER

Chair Vagnozzi called the meeting to order at 6:00pm

2. APPROVAL OF THE AGENDA

VICE-CHAIR LOVELL ASKED THAT THE AGENDA BE AMENDED TO ADD ITEM 7.B. 2045 METROPOLITAN TRANSPORTATION PLAN. MOTION BY COMMISSIONER POTTER TO APPROVE THE AMENDED AGENDA. SUPPORTED BY COMMISSIONER DESCHAIINE. MOTION PASSES 6-0.

3. APPROVAL OF THE MINUTES-OCTOBER 22, 2020

MOTION BY COMMISSIONER DESCHAIINE TO APPROVE THE MINUTES. SUPPORTED BY VICE-CHAIR LOVELL. MOTION PASSES 6-0.

4. COMMUNICATIONS

Chair Vagnozzi mentioned he had asked that House Bill 4733 be included in the packet. He thought the changes to the procedures for setting speed limits could hinder the Township. Chief Plaga stated the changes should not affect how the Police Department works with the Ingham County Road Department on modifying speed limits.

5. PUBLIC REMARKS – NONE

6. 2021 MEETING SCHEDULE

MOTION BY COMMISSIONER DESCHAIINE TO APPROVE THE 2021 MEETING SCHEDULE WITH THE AMENDED MEETING TIMES OF 7:00 PM FOR VIRTUAL MEETINGS AND 6:00 PM FOR THE IN-PERSON MEETINGS. SUPPORTED BY COMMISSIONER ROBERTSON. MOTION PASSES 6-0.

(2021 Meeting Schedule Resolution on file.)

7. COMMISSION DISCUSSION

A. Capital Area Transportation Authority (CATA) Update

Chair Vagnozzi welcomed Andrew Brieschke and Craig Frazier.

The Commission was provided with information on Redi-Ride service in the Township for September 2018 to October 2020. The information included passenger type, booking analysis, extended hours ridership and student ridership. (A copy of the information is on file.)

- March 23, 2020 due to COVID-19 CATA suspended all service except for essential trips such as medical.
- Full schedule for fix-route and Redi-Ride started again on May 26, 2020.
- Prior to March the approximate ridership per day for all services was 55,000.
- In March ridership dropped to 1,000-1,200 rides per day.
- As of October ridership is slowly increasing to approximately 8,000 rides per day.
- Redi-Ride ridership is at 50 percent capacity and fix-route ridership is at 15 percent capacity.
- CATA has worked with Ingham County Health Department to establish safety protocol.
- Driver is separated from riders and must wear a mask.
- Riders must wear a mask “no mask no ride”.
- CATA staff to work with Township staff to inform residents of the safety protocol CATA has put in place and what they are doing to keep riders safe.
- Student ridership on Redi-Ride is down due to schools being closed or choosing not to use Redi-Ride due to COVID-19.
- In September 2019 Redi-Ride service was increased to 3 full-time runs and 2 part-time runs Monday-Friday. On Saturday service was increased to 2 full-time runs and 1 part-time run.
- In February 2020 the start time in the morning went from 7:30 a.m. to 7:00 a.m. The half-hour increase has helped meet the demand in the morning.
- From September 2019 through February 2020 there were no capacity denials. Client refusals, cancelations and no-shows were reduced.
- The increased level of service has proven to be appropriate and has provided trips with reduced ride time. CATA has not received any complaints since the increased service has started.
- Veterans can now ride free on Redi-Ride or fix-route.
- CATA staff will provide information on new, upgraded or moved bus shelters in the Township.

B. 2045 Metropolitan Transportation Plan

Chair Vagnozzi suggested that Commissioners wishing to comment on the plan provided those to staff by November 30, 2020. Staff will incorporate comments into a letter to be sent to Tri-County Board of Commissioners.

8. COMMISSION COMMENTS

Chair Vagnozzi welcomed Joshua Robertson to the Commission.

Commissioner Deschaine suggested the Chair or Vice-Chair make a presentation to the Township Board at their meeting on December 8 to update the Board on Redi-Ride.

9. NEXT MEETING DATES

A. Township Board Meeting December 8, 2020.

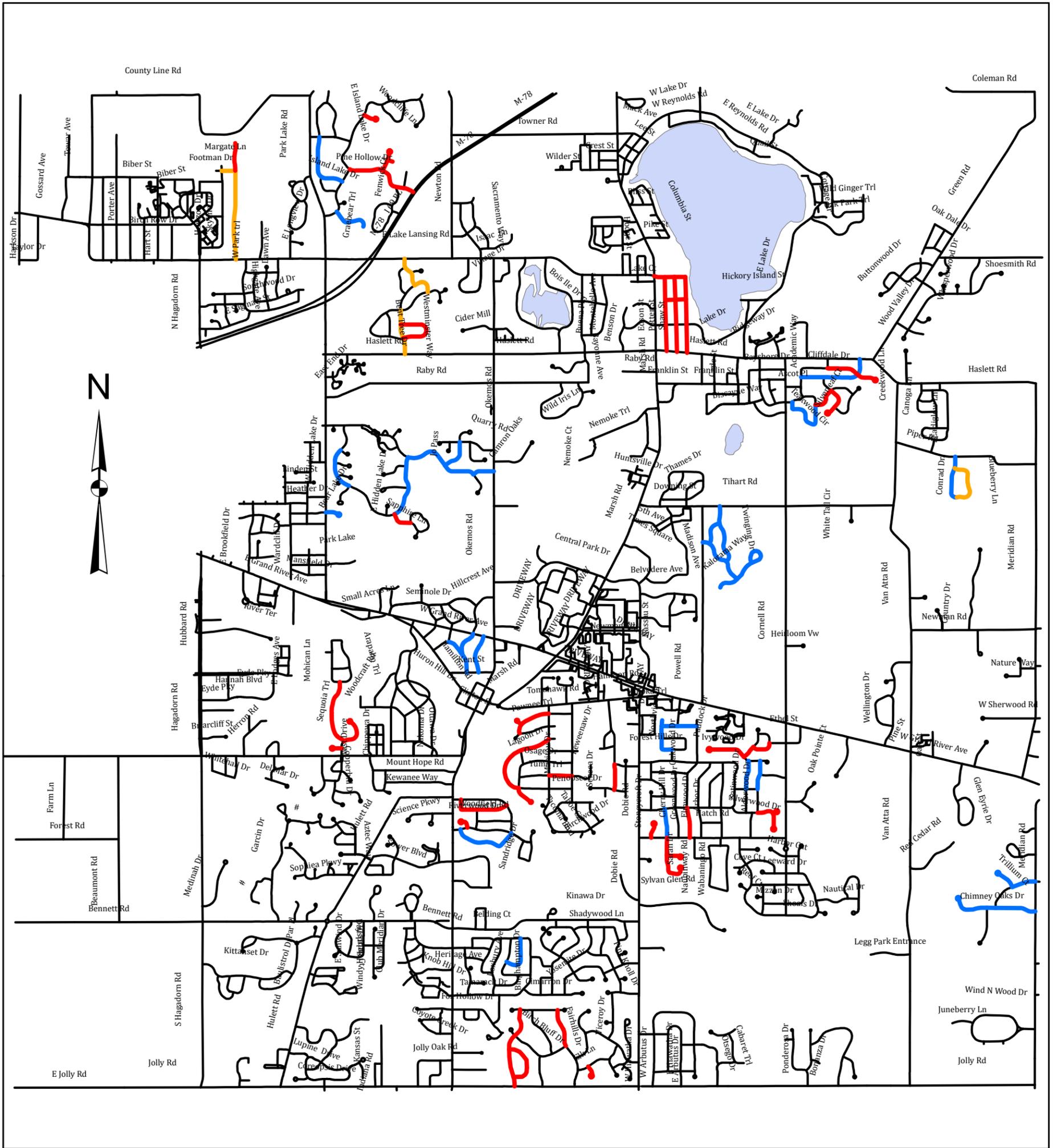
B. Transportation Commission Meeting January 21, 2021

10. ADJOURNMENT

**MOTION BY COMMISSIONER DESCHAIINE TO ADJOURN. SUPPORTED BY VICE-CHAIR
LOVELL MOTION PASSES 6-0.**

Meeting adjourned at 7:36 p.m.

MERIDIAN TOWNSHIP 2021 LOCAL ROAD CONSTRUCTION



2021 Local Road Construction

- 2021 Resurfacing - 8.87mi
- 2021 Cape Seal - 1.50mi
- 2021 Crack Fill - 7.61mi

2021 Local Road Program

Resurfacing

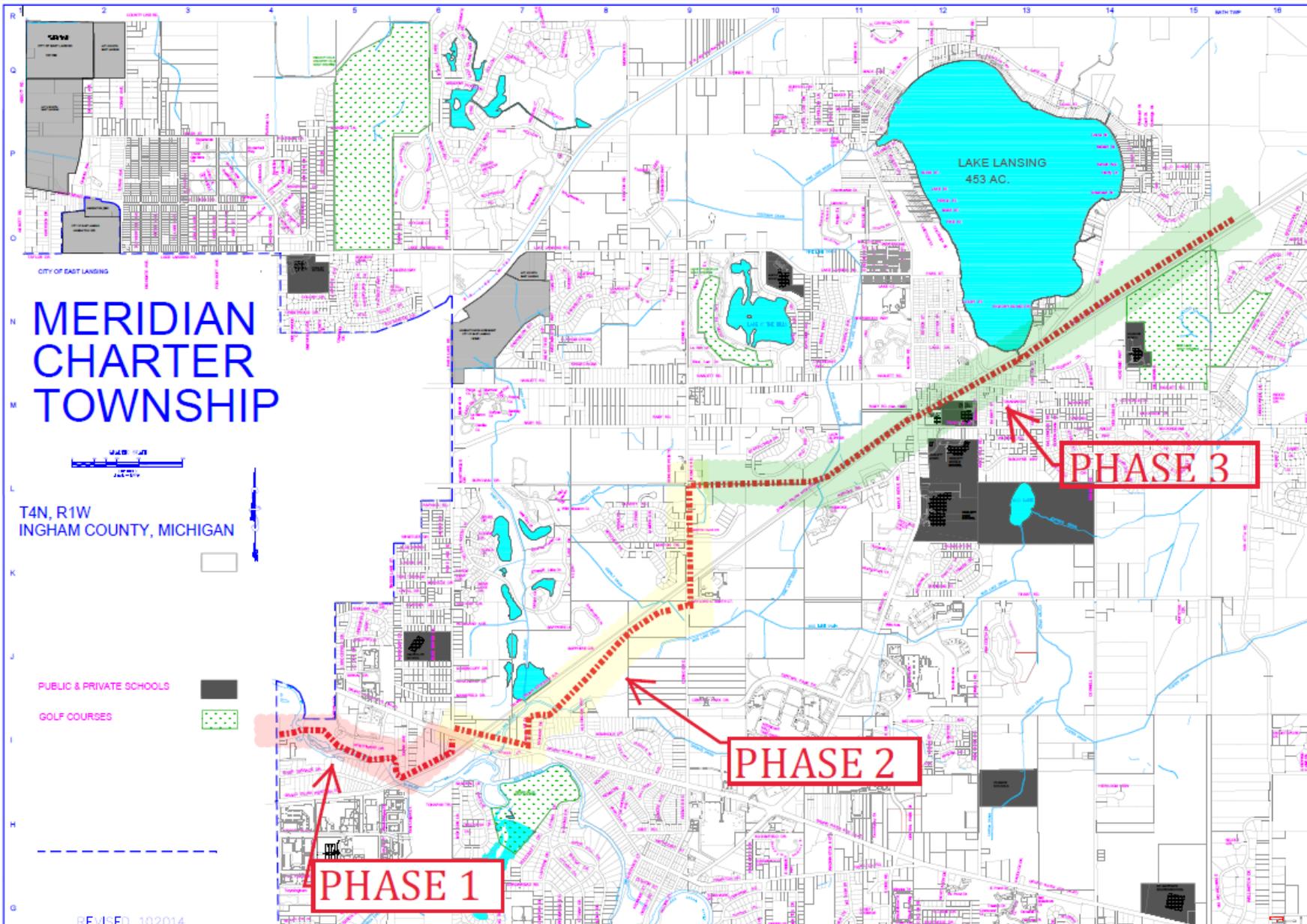
Street	From	To
Skyline	Margate	End
Pine Hollow	Saginaw	Whitehills Lakes
Mereford	Pine Hollow	End
Fenwick	Mereford	End
Overglen	Pine Hollow	End
Kingcross	Westminster	Westminster
Shaw	Haslett	Park (Lake Lansing)
Potter	Haslett	Park (Lake Lansing)
Edson	Haslett	Park (Lake Lansing)
Colby	Edson	Shaw
Park (Lake Lansing)	Marsh	Shaw
Cliffdale	Hillview	Cul-de-sac End
White Ash	Buckingham	Cul-de-sac End
Buckingham	Teakwood	White Ash
Sapphire	East Hidden Lake	Sapphire Lane T
Sequoia	Comanche	Mohican/Arapaho
Comanche	Sequoia	Cul-de-sac End
Cochise	Comanche	Cul-de-sac End
Tomahawk	Tacoma	Pawnee
Tomahawk Circle	Tomahawk	Cul-de-sac End
Lagoon	Tacoma	Wausau
Wausau	Lagoon/Yuma	Cul-de-sac End
Osage	Tacoma	Manitou
Seneca	Shaker	Mirabeau
Ivywood	Mistywood	Cul-de-sac End
Mistywood	Ivywood	Spicewood
Spicewood	Mistywood	Ethel
Silverwood	Alderwood	Cul-de-sac End
Shortwood	Silverwood	Cul-de-sac End
Oakwood	Hatch	Birch
Indian Glen	Hatch	Cul-de-sac End
Mojave	Indian Glen	Cul-de-sac End
Farm Meadows	Hatch	Cul-de-sac End
Cherrywood	Riverwood	Butternut
Butternut	Cherrywood	Riverwood
Confier Circle	Woodfield	Cul-de-sac End
Autumnwood	Jolly	Belwood
Timberview	Autumnwood	Autumnwood
Fairhills	Birch Bluff	Belwood
Rainbow Court	Elk	Cul-de-sac End

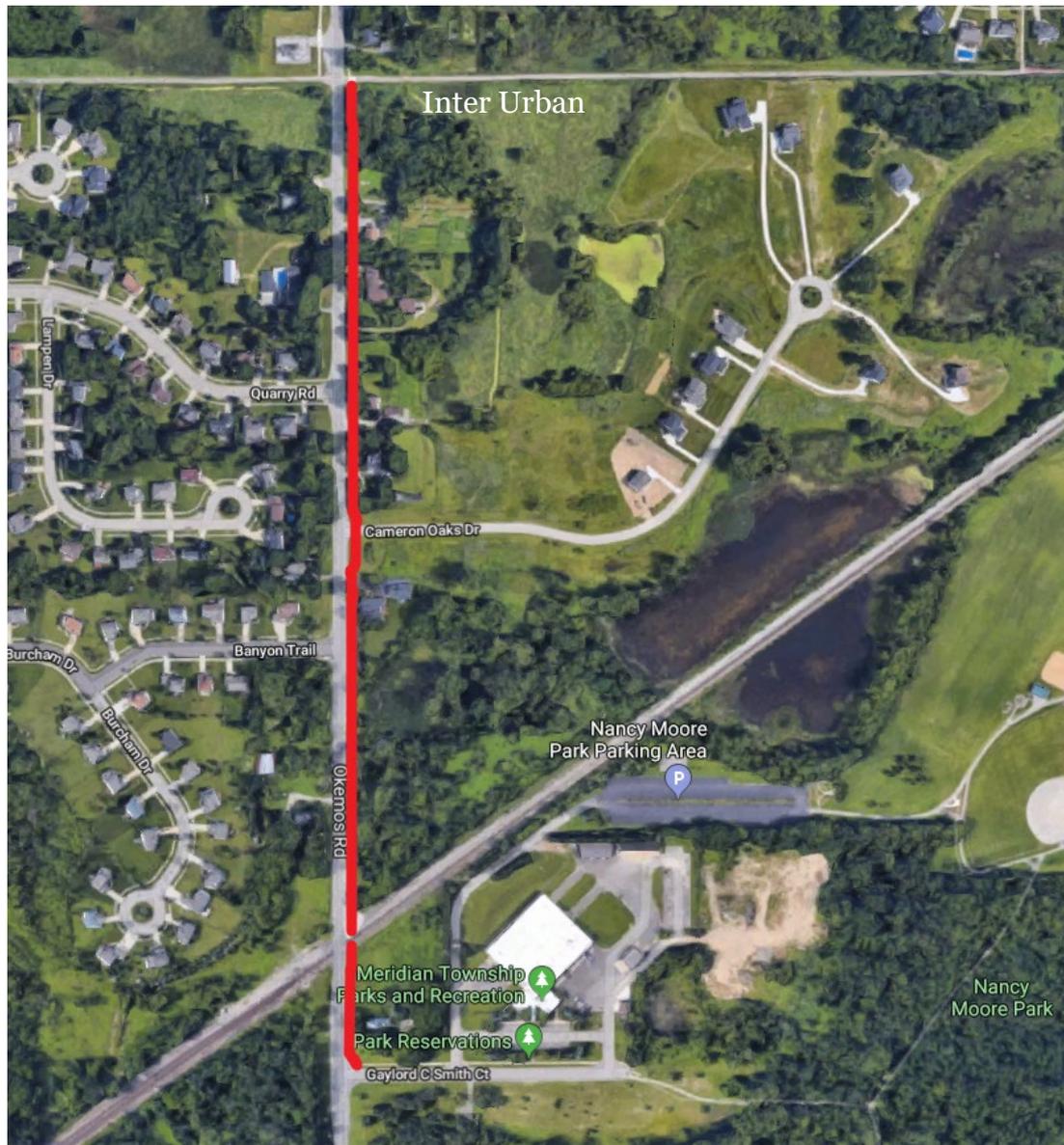
Preservation- Crack Fill

Street	From	To
Island Lake	Whitehills Lakes	Meadow Wood
Southridge	Whitehills Lakes	~6069 Southridge
Woodside	Haslett	Ventura Place
Teakwood	Buckingham	Buckingham
West Hidden Lake	Bruin	Bear Lake
East Hidden Lake	Burcham	Saphire
Burcham	East Hidden Lake	Okemos
Jo Pass	Burcham	Quarry
Heather Circle	Park Lake	Cul-de-sac End
Twinging	Tihart	Giesboro
Anacostia	Twinging	Giesboro
Giesboro	Powell	Kalorama
Kalorama	Cul-de-sac	End
Conrad Drive	Piper	~5318 Conrad
Kenmore	Kent	Grandview
Hillcrest	Grand River	Kenmore
Ardmore	Grand River	Kent
Hillside	Marlborough	Oakwood
Marlborough	~4508 Marlborough	End
Satinwood	Ivywood	Silkwood
Alderwood	Ivywood	Silkwood
Indian Glen	Birchwood	Hatch
White Owl	Woodfield	Woodfield
Sheldrake	Roxbury	Belding Court
Victoria Hills	Meridian	Trillium
Trillium	Victoria Hills	Cul-de-sac End
Chimney Oaks	Meridian	Cul-de-sac End

Preservation- Cape Seal

Street	From	To
Skyline	Lake Lansing	Margaret
Margaret	Skyline	End
Everett	Lake Lansing	Barnsbury
Westminster	Barnsbury	Haslett
Blueberry	Conrad	Conrad





Phase 2b Proposed Route

PUBLIC UTILITIES

- | | |
|---|---|
| AT&T
337 N. ABBOTT, RM. 201
EAST LANSING, MI 48823
517.337.3660 | COMMUNICATIONS |
| CONSUMERS ENERGY
530 W. WILLOW ST.
P.O. BOX 30162
LANSING, MI 48909
517.373.6100 | GAS / ELECTRIC |
| COMCAST
1070 TROWBRIDGE ROAD
EAST LANSING, MI 48823
517.332.1012 | COMMUNICATIONS |
| MERIDIAN CHARTER TOWNSHIP
5151 MARSH RD.
OKEMOS, MI 48864
517.853.4440 | WATER MAINS
SANITARY SEWER
PATHWAYS |
| WOLVERINE PIPE LINE COMPANY
8105 VALLEYWOOD LANE
PORTAGE, MI 49024-5251
231.323.2491 | PETROLEUM PIPELINE (8") |
| INGHAM COUNTY DRAIN COMMISSIONER
707 BUHL ST.
MASON, MI 48854
517.676.8395 | DRAINS & STORM SEWER |
| INGHAM COUNTY ROADS DEPARTMENT
301 BUSH ST.
MASON, MI 48854
517.676.9722 | PUBLIC ROADS AND
RIGHT OF WAYS |



SHAW STREET COUNTY PARK CONNECTOR CONSTRUCTION PLANS for the CHARTER TOWNSHIP OF MERIDIAN INGHAM COUNTY, MICHIGAN

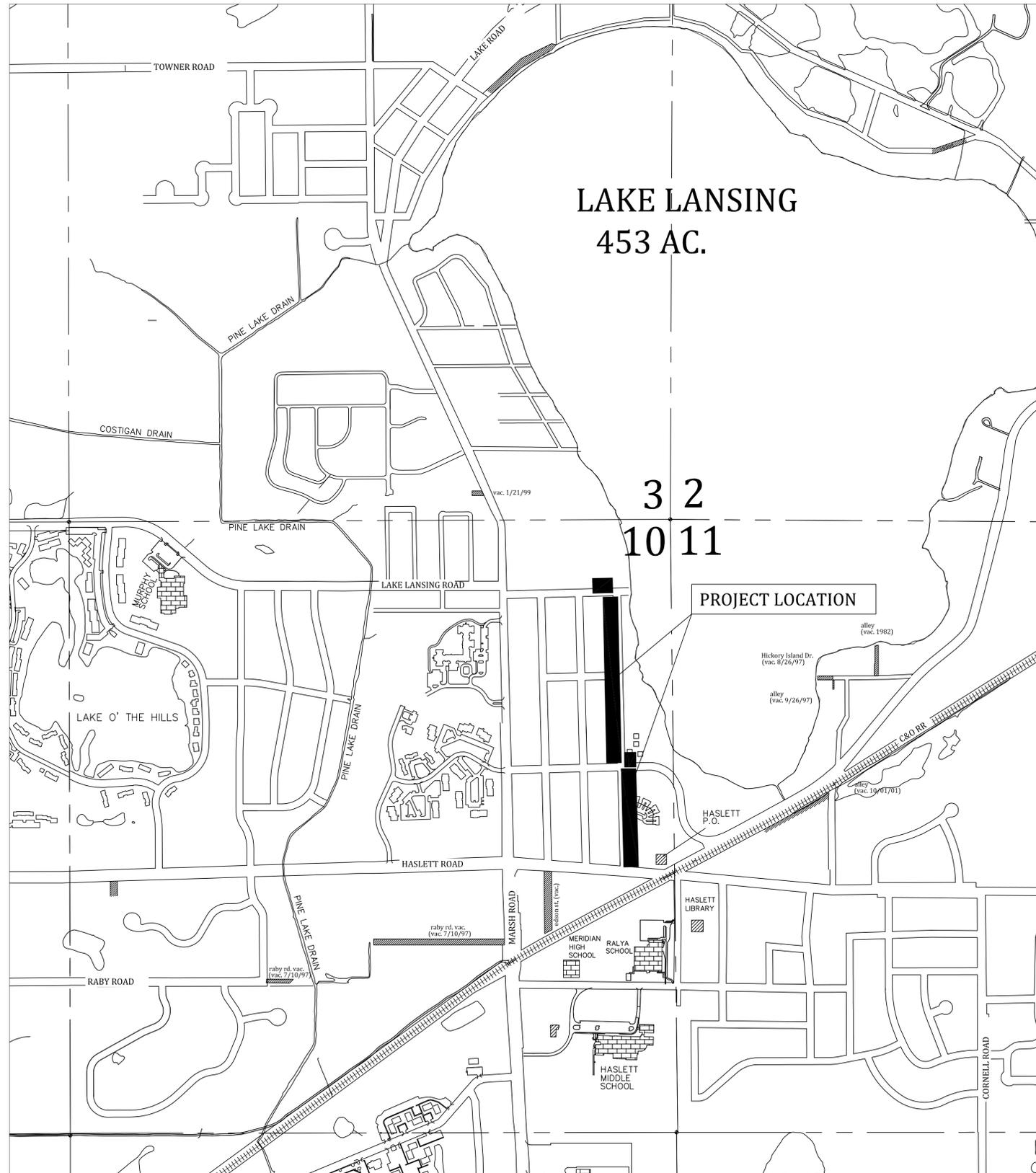
RECOMMENDED CONSTRUCTION SCHEDULING & SEQUENCING	DECEMBER	JANUARY	FEBRUARY	MARCH	APRIL	MAY	JUNE	JULY	AUGUST	SEPTEMBER	OCTOBER	NOVEMBER
PROPOSED TRAIL REMOVE TREES												
PROPOSED TRAIL PLACE AND MAINTAIN SESC MEASURES												
PROPOSED TRAIL PREP AND GRADE FOR TRAIL												
PROPOSED TRAIL INSTALL TRAIL												
PROPOSED TRAIL RESTORATION												
PROPOSED TRAIL FINAL INSPECTION												

SOIL EROSION & SEDIMENTATION CONTROL NOTES

- ALL EROSION AND SEDIMENT CONTROL WORK SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF THE INGHAM COUNTY DRAIN COMMISSION AND MERIDIAN TOWNSHIP
- DAILY INSPECTIONS SHALL BE MADE BY THE CONTRACTOR FOR EFFECTIVENESS OF EROSION AND SEDIMENT CONTROL MEASURES, AND NECESSARY REPAIRS SHALL BE PERFORMED WITHOUT DELAY
- EROSION OF ANY SEDIMENT FROM WORK ON THE SITE SHALL BE CONTAINED ON-SITE AND NOT ALLOWED TO COLLECT ON ANY OFF-SITE AREAS OR IN WATERWAYS. WATERWAYS INCLUDE BOTH NATURAL AND MAN-MADE OPEN DITCHES, STREAMS, STORM DRAINS, LAKES, PONDS, AND WETLANDS
- THE CONTRACTOR SHALL APPLY TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES WHEN REQUIRED AND AS DIRECTED ON THESE PLANS. THE CONTRACTOR SHALL REMOVE TEMPORARY MEASURES AS SOON AS PERMANENT STABILIZATION OF SLOPES, DITCHES, AND OTHER CHANGES HAVE BEEN ESTABLISHED
- STAGING THE WORK SHALL BE DONE BY THE CONTRACTOR AS DIRECTED IN THESE PLANS AND AS REQUIRED TO ENSURE PROGRESSIVE STABILIZATION OF DISTURBED EARTH
- SOIL EROSION CONTROL PRACTICE WILL BE ESTABLISHED IN EARLY STAGES OF CONSTRUCTION BY THE CONTRACTOR. SEDIMENT CONTROL PRACTICES WILL BE APPLIED AS A PERIMETER DEFENSE AGAINST ANY TRANSPORTING OF SOIL OFF THE SITE
- THE CONTRACTOR SHALL PRESERVE NATURAL VEGETATION AS MUCH AS POSSIBLE
- VEGETATION STABILIZATION OF ALL DISTURBED AREAS SHALL BE ESTABLISHED WITHIN 15 DAYS OF COMPLETION OF THE FINAL GRADING

SOILS:

- MUD LAKE DRAIN:**
- Hn - HOUGHTON MUCK
 - UeB - URBAN LAND - BOYER - SPINKS COMPLEX (0 - 10% SLOPES)
 - UpA - URBAN LAND - CAPAC - COLWOOD COMPLEX (0 - 4% SLOPES)
 - UeB - URBAN LAND - MARLETTE COMPLEX (2 - 12% SLOPES)



STANDARD CONSTRUCTION NOTES

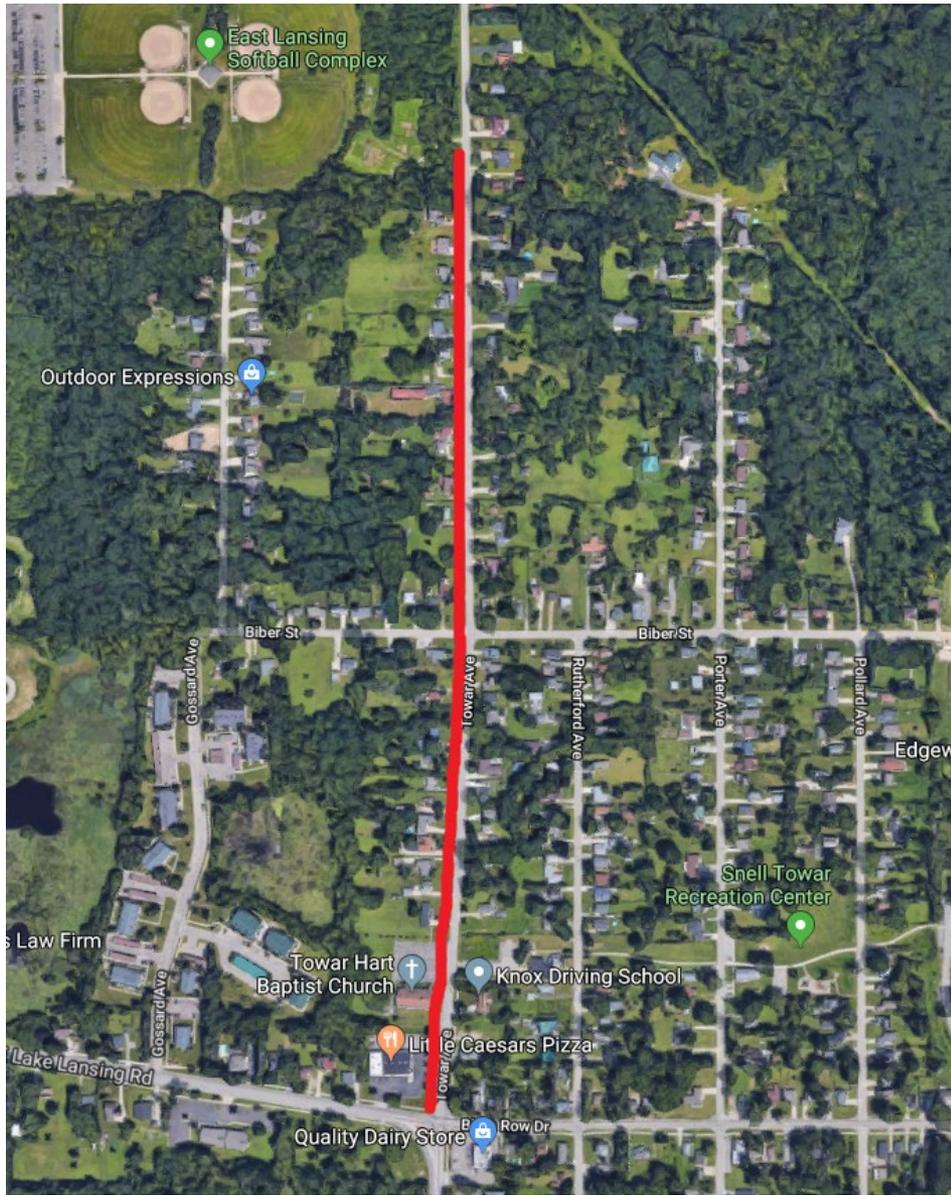
- The Contractor shall notify the Charter Township of Meridian, Department of Public Works, Office of Engineering 517-853-4440 a minimum of 72 hours prior to the start of construction of public utilities or of construction within the public right-of-way.
- All construction shall conform to the current standards and specifications of the Charter Township of Meridian which are included as part of these plans in effect at the time of construction.
- After the completion of construction of public utilities or construction within public right-of-way, the contractor must request a final inspection. Any punchlist items resulting from the final inspection must be resolved prior to final release and acceptance.
- The existing utilities indicated on the plans are in accordance with available information. It shall be the contractor's obligation to verify the exact location of all existing utilities, which might affect this job.
- The contractor shall notify "MISS DIG" 1-800-482-7171 at least 72 hours prior to the start of construction.
- The contractor shall at all times be aware of inconvenience caused to the abutting property owners and the general public. Where the contractor does not remedy undue inconveniences, the Charter Township of Meridian, upon four hours notice, reserves the right to perform the work and deduct the cost therefrom from the money due the contractor.
- A Registered Land Surveyor provided by the contractor at the contractor's expense shall replace all property irons and monuments disturbed or destroyed by the contractor's operations.
- Contractor shall provide Owner and Township Engineer a copy of written permission to use private property for storage of equipment and materials or for his construction operations.
- Trench backfill under existing or proposed roadways, driveways, and parking areas, shall be sand or gravel, placed in 12" layers (maximum) and consolidated to 95% of maximum density as measured by modified proctor unless otherwise noted.
- Trees and shrubs are to be protected during construction and bored where necessary.
- Existing fences shall be removed and restored to their original condition or better where in conflict with construction.
- Driveways, culverts, ditches, drain tile, tile fields, drainage structures, etc., that are disturbed by the contractor's operations shall be immediately restored.
- All established lawn areas disturbed by the contractor's operations shall be resodded with matching sod. All other areas shall be seeded and mulched. Seeding and mulching shall be done in accordance with the General Specifications.
- All ditch slopes shall have established vegetation and be protected from erosion.
- All utility poles in close proximity to construction shall be supported in a manner satisfactory to the utility owner.
- Onsite parking and sanitary facilities shall be provided for construction workers. The facilities shall be constructed and operated (with minimal impact to the surrounding area) to the satisfaction of the Township.

PATHWAY NOTES

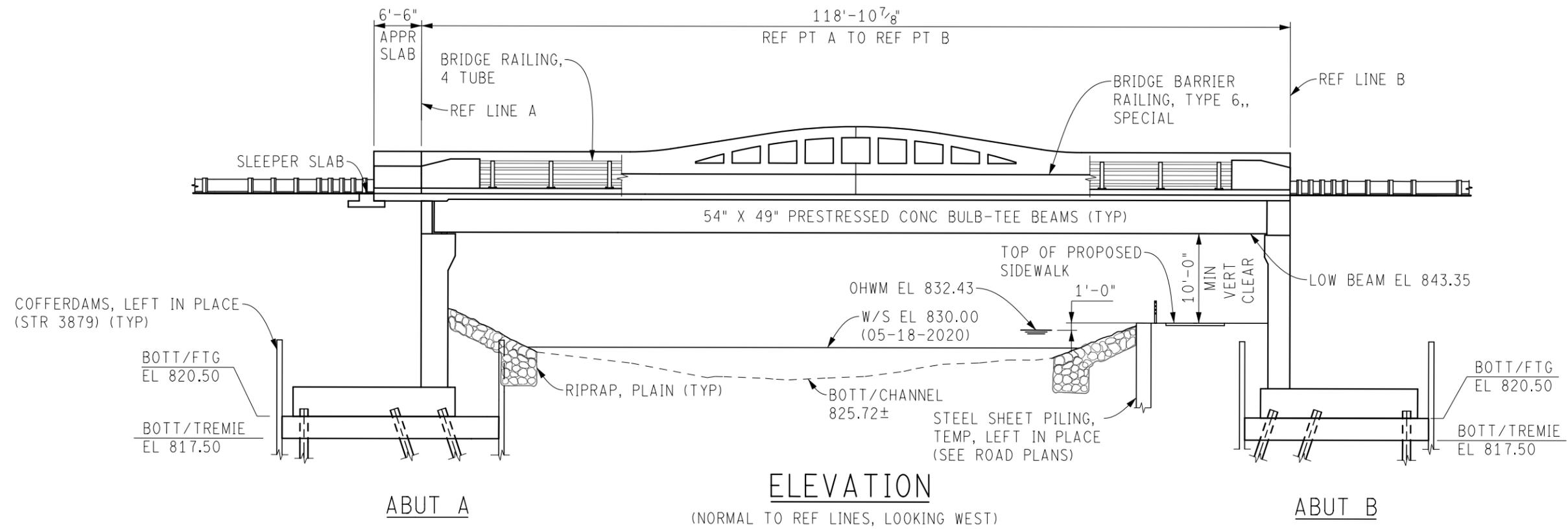
- Pathways and sidewalks shall be four (4) inch thick concrete except at driveways where they shall be six (6) inch (residential) or seven (7) inch (commercial) thick concrete.
- Three (3) inches of compacted sand base shall be placed under all pathways and sidewalks.
- All bituminous aprons shall be two and one-half (2½) inches thick, unless otherwise noted.
- Property irons shall be maintained by the Contractor.
- All existing concrete and bituminous to be removed shall be sawcut. All bituminous removal shall be considered incidental to construction.
- All aggregate base material shall be four (4) inches of 22A.
- All tree (less than 6") and shrub removal shall be considered part of subgrade preparation.
- Location of new plant material shall be as directed by the Engineer, and shall be installed in accordance with guidelines established by the A.N.L.A.
- All plant material not marked for removal shall be protected.
- Bituminous drives shall be sawcut 18" on either side of proposed pathway.
- The maximum longitudinal slope is 5% (up to an absolute maximum of 8½% at the direction of the Engineer) and the maximum cross slope is 2%.
- Expansion joints shall be placed at approximately 100' intervals and shall be ½" thick. Contraction joints shall be sawcut to a depth of ¼ of the depth of the concrete.
- All lumber to be pressure treated (Osmose 33 or equal) to 0.4 retention.
- All items not covered under a specific pay item shall be considered incidental.

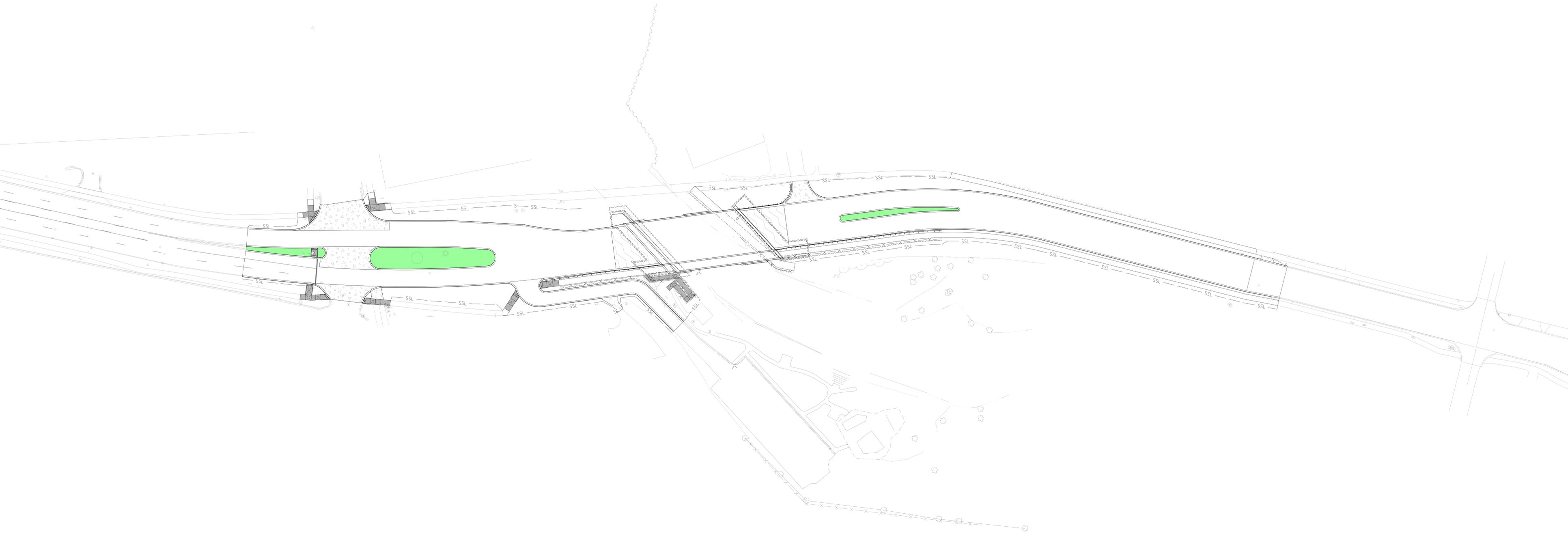
SHEET INDEX

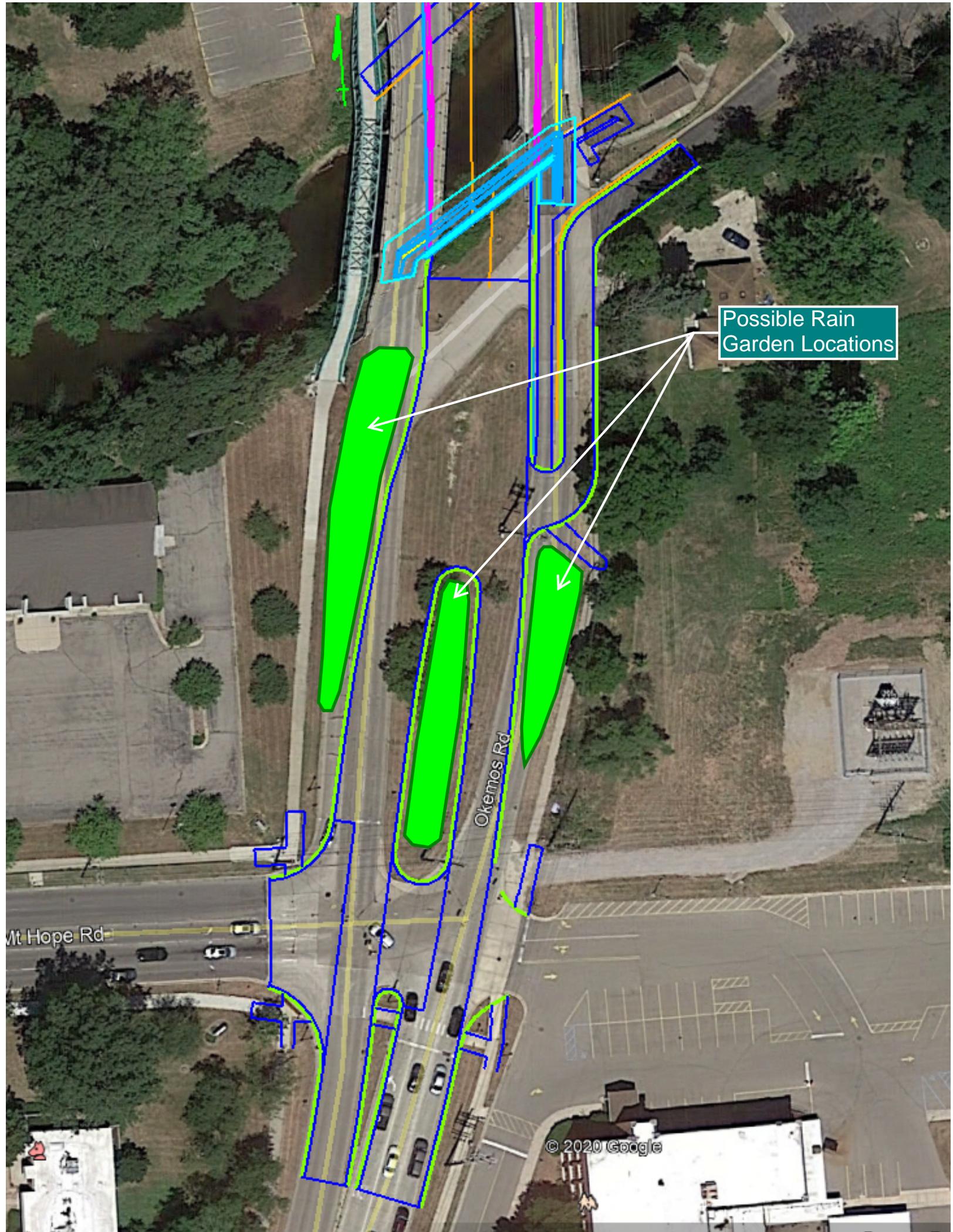
SHT #	TITLE
1	COVER SHEET
2	SOUTH END TRAIL - PLAN STA 0+00 TO 25+00
3	PATHWAY DETAIL SHEET



Proposed Route







Possible Rain Garden Locations

Okemos Rd

Wt Hope Rd

© 2020 Google

Rain Garden Concepts







08 Dec Winter Is Coming: 6 Ways Local Leaders Can Help People Warmly and Safely Social Distance Outdoors During COVID-19

Posted at 14:29h in Insight by 8 80 Cities Team ·

This article is a collaboration between 8 80 Cities and [AARP Livable Communities](#).

Communities across the globe have adapted to the COVID-19 pandemic by re-imagining social gatherings — moving dining and other activities outdoors, for instance — where open spaces make social distancing easier. But winter is coming — or, in many places, has already arrived.

“We need to find new and creative ways to connect with one another outdoors where we can physically distance,” says **Amanda O’Rourke**, executive director of **8 80 Cities**, a livability and public spaces advocacy organization — and frequent **AARP Livable Communities** partner — based in Toronto, Canada. “This is especially true for older adults, who are among the most susceptible to COVID, the most likely to report feeling socially isolated and the most likely to remain indoors during winter.”



Although social isolation tends to increase for everyone during the winter, O’Rourke asserts, “we are misdiagnosing the issue by blaming winter. The problem is much deeper than that. Public spaces and transportation systems that are already forces for exclusion and social isolation become even more problematic in winter.”

“The way we design, plan and manage our outdoor public spaces do very little to invite vibrant winter activity and public life,” she explains. “That’s the root cause, which, combined with the impacts of COVID-19, creates a perfect storm for mental and physical health crises. The good news is that winter can be an asset to communities if leveraged properly.”

According to O’Rourke and **David Simor**, a senior project manager at 8 80 Cities, local governments can make winter spaces inclusive and inviting for people of all ages by addressing the challenges caused by cold temperatures, winter light and accessibility and by “reframing the conversation about winter.” Their suggestions:

1. Clear sidewalks, bike lanes, and walking paths of snow and ice

Sidewalks are public spaces that improve mobility, accessibility and quality of life. Yet public works departments prioritize the clearing of snow and ice from roads while generally neglecting sidewalks. In fact, many cities don't bother to clear sidewalks at all. Why devalue these important assets by leaving them inaccessible for three or four months of the year? Cities with accessible, active transportation networks — including sidewalks and bike paths — see high levels of physical activity and social connection in winter. Keeping sidewalks walkable and streets or paths bikeable during the COVID-19 pandemic can relieve crowding on public transit systems.

2. Make outdoor lighting “human scale”

For many people, one of the hardest parts of winter is the lack of sunlight. The decrease in daylight hours contributes to a higher rate of traffic collisions during the winter months. To combat the hazards, many cities increase the wattage of lighting along streets, sidewalks and public spaces. Some use lighting in fun ways by hosting festivals, competitions and art installations that brighten the night.

Prioritize the Most Vulnerable

People experiencing homelessness or who are housing insecure need a way to get out of the cold. The pandemic has placed already overwhelmed shelter and support systems under even more duress. Local governments can support the under-housed by contracting with local hotels that have excess vacancies as well as by using publicly owned assets — such as libraries, community centers and civic buildings — as winter shelters and temporary housing.

3. Design for winter by creating comfortable “micro-climates”

Being cold isn't fun. Warmth is an essential ingredient for creating the winter-friendly public places so many communities need. Local governments, businesses and organizations can plan and design spaces for winter by installing warming zones and fire pits in places where people

normally congregate for physical activity, such as parks and plazas, as well as in locations where people typically have to wait outdoors, such as transit stops. Such micro-climates can raise the surrounding air temperature by up to 15 degrees and both encourage and enable people to spend more time outdoors. When closed to vehicle traffic, street spaces can support social, physical and commercial activity through the use of heated patios and [parklets](#).

4. Use snow and ice to a community's advantage

The [River Trail](#) in Winnipeg, Manitoba, Canada, and the [Rideau Canal](#) in the nation's capital city of Ottawa, Ontario, provide distinctive skating and recreational opportunities that connect large parts of each city. Communities with beautiful trail systems that teem with people in the summer can support safe recreational opportunities for walking, cross-country skiing, and fat-tire bicycling in winter. [Leadville, Colorado](#), closes its downtown streets for cold-weather activities such as ski-joring (a sport in which a person on skis is pulled by a horse, dog or motor vehicle). Other cities close streets for tobogganing or [Open Streets](#) programs. Wintertime also creates opportunities for snow building and ice sculpture competitions.

5. Maintain park amenities

Properly maintained walking paths can boost physical activity levels for park users during the winter. But in many North American cities, parks are closed, or essentially closed, once cold weather arrives for the season. Restrooms are locked, paths aren't cared for and event programming (see item 6) comes to a halt. Winterizing park restrooms is even more important during a pandemic when people have fewer options for accessing indoor washrooms (such as in libraries, community centers, stores or eateries). Older adults and families with young children are much less likely to visit a park or outdoor space if safe, clean toilets aren't available and accessible. Places that invite people to spend time outside need to provide comfortable and clean restrooms.

"Being cold isn't fun. Warmth is

6. Provide winter programs and activities

Another reason parks and public spaces see significantly fewer visitors during colder months is because many municipalities don't host events and activities in winter.



Programming that would typically occur inside libraries or community centers can be moved outdoors. The activities that work best are ones that encourage people to move their bodies for both exercise and warmth, such as yoga, walking, tai-chi, dancing, golf and simple games for people of all ages to enjoy. It doesn't take much effort or money to enable people to be active and healthy in winter. Following are just a few of the creative ways communities can woo people outdoors in winter:

- Host pop-up hot chocolate stands
- Install [Little Free Library](#) book and grocery giveaway boxes on lawns
- Place "wishing trees" and "kindness rocks" in neighbourhoods
- Organize volunteer snow-shovelling crews

For many people, the appetite for connecting with others has become stronger due to the forced isolation caused by the pandemic.

Communities and local leaders can be catalysts and facilitators in support of socializing and social distancing in the cold — and during the winter, mask-wearing for avoiding infection provides the added benefit of warmth.

Page published December 2020 | A similar version of this article also appears on: [AARP Livable Communities](#).