



AGENDA
CORRIDOR IMPROVEMENT AUTHORITY
July 25, 2018 6:00 pm
Central Fire Station

1. CALL TO ORDER
2. MISSION: The Corridor Improvement Authority of the Charter Township of Meridian is dedicated to establishing the Grand River Avenue corridor in Meridian Township as a prime business and recreation destination in the Tri-County region. The Authority is focused on creating programs and projects that invigorate the streetscape and build partnerships among corridor property owners, business owners, and residents.
3. APPROVAL OF AGENDA
4. CONSENT AGENDA
 - a. Minutes – May 23, 2018
 - b. Development Projects Update
5. PUBLIC COMMENT
6. PRESENTATION – None
7. NEW BUSINESS
 - a. Panera Project Review
8. OLD BUSINESS
 - a. Corridor Plan – Goals, Objectives, Timelines, Responsible Parties, Budget
9. MERIDIAN MALL REPORT
10. TOWNSHIP BOARD REPORT
11. CHAIR REPORT
12. STAFF REPORT
13. OPEN DISCUSSION/BOARD COMMENTS
14. PUBLIC COMMENT
15. NEXT MEETING DATE:
 - a. Next Meeting Date: August 22, 2018 - 6:00 p.m. Central Fire Station
16. ADJOURNMENT



CHARTER TOWNSHIP OF MERIDIAN
CORRIDOR IMPROVEMENT AUTHORITY-CENTRAL FIRE STATION
5000 OKEMOS ROAD, OKEMOS, MI 48864
WEDNESDAY, MAY 23, 2018 6:00 pm – Minutes (Draft)

MEMBERS

PRESENT: Eric Foster, Bruce Peffers, Supervisor Ron Styka, Barry Goetz, Chris Rigterink, Kellie Johnson, and Jeff Ross

MEMBERS

ABSENT: Chris Nugent, Brian Jones, and Todd Huhn

STAFF

PRESENT: Economic Development Director Chris Buck

OTHERS

PRESENT: Kirk Byrens

1. CALL MEETING TO ORDER

Kellie Johnson called the meeting to order at 6:04 pm and read the mission statement.

2. MISSION: The Corridor Improvement Authority of the Charter Township of Meridian is dedicated to establishing the Grand River Avenue corridor in Meridian Township as a prime business and recreation destination in the Tri-County region. The Authority is focused on creating programs and projects that invigorate the streetscape and build partnerships among corridor property owners, business owners, and residents.

3. APPROVAL OF THE AGENDA

MOTION BY SUPERVISOR STYKA TO APPROVE AGENDA. SUPPORTED BY MEMBER PEFFERS. MOTION APPROVED 7-0.

4. CONSENT AGENDA

MOTION BY MEMBER GOETZ TO APPROVE CONSENT AGENDA. SUPPORTED BY MEMBER FOSTER. MOTION APPROVED 7-0.

5. PUBLIC COMMENT

Kirk Byrens introduced himself. He is the local State Farm agent located by Panera and lives in Okemos. He moved to Okemos from Marshall about a year ago and wants to serve the community as a means of giving back and learning the area better.

6. PRESENTATION

Corridor Improvement Authority Minutes (Draft)
May 23, 2018
Page 2

Sanju Guinn from Consumers Energy presented the following:

- 3 types of light fixtures - Cobrahead, Post Top, Center Suspension
- 3 types of lighting sources - Mercury Vapor, High Pressure Sodium, LED
- Free to replace MV with HPS on Center Suspension
- Free to replace MV with LED on Cobraheads
- Free to replace MW with LED on Post Top
 - A total of 479 free upgrades for the Township which will save on energy costs. In addition, we are getting a price to upgrade another 1,648 lights.
 - Director Perry in Public Works is working on this project. It will not be isolated to the Corridor, but the CIA should feel good that these free upgrades, cost savings and overall assessment started because of the CIA's request.
 - Consumers Energy is working on the locations of all the lights.
 - Director Buck will continue to keep the CIA informed on the project progress.

7. NEW BUSINESS

A. Corridor Action Plan

Director Buck led a discussion about the CIA needing to develop a series of goals, actions steps and timelines to accomplish them. Members discussed the excerpts from The Capitol Corridor Book that was included in the board packet and how it expresses the results of research, studies and feedback by citizens. While the underlying theme is around CATA's project and covers other municipalities as well as Meridian, there is a lot of visioning for Meridian that is relevant. The Board voted 7-0 to have Director Buck read the passages more thoroughly and report back a list of topics to discuss at future CIA meetings, resulting in these action steps.

8. OLD BUSINESS

A. MUPUD Zoning Amendment Update

Director Buck reported that the MUPUD zoning amendment for Okemos and Haslett passed allowing for more height and density in development.

B. Form Based Code/Shaping the Avenue Update

The Planning department received questions and feedback from the Planning Commission after the work session in late April. Staff will work with the Shaping the Avenue Consultants over the summer and the Township will assess the plan again in August.

8. MERIDIAN MALL REPORT

Member Huhn was absent, but Director Buck relayed that the CBL Mall senior leadership met with Township staff last week and discussed the Farmers' Market and developing a master plan of sorts for the mall's future. The Township will work closely with them on their vision. The CIA may be able to play a role in this soon so stay tuned.

Corridor Improvement Authority Minutes (Draft)
May 23, 2018
Page 3

9. TOWNSHIP BOARD REPORT

Supervisor Styka echoed the MUPUD amendment and it resulting in the Douglas J press conference tomorrow. The Board is hopeful that this will be the first step in a major update to our most depressed area.

10. CHAIR REPORT

None.

11. STAFF REPORT

Director Buck reported on the following items:

- Douglas J Press Conference tomorrow at 10:30am.
- Kirk Byrens & Lisa Kost are interested in joining the CIA; however, the CIA is at capacity according to the bylaws.
- Recent ribbon cuttings listed on Development Projects in packet.
- Elevation party June 19th 4:00pm-8:00pm, invite coming for CIA.
- Baskin Robbins Open House June 2nd.
- The Red Cedar Manor project, along the Corridor, just submitted an application. The CIA members agreed that all projects within the corridor should be in the board packets so the CIA can decide if they want to support, oppose or remain neutral on each project.
- Director Buck will bring the Red Cedar Manor and Panera plans to the next meeting for discussion.
- The Drive in Theater on Saginaw was rezoned to Commercial-2, stay tuned for development project.

12. OPEN DISCUSSION/BOARD COMMENTS

None.

13. PUBLIC COMMENT

None.

14. NEXT MEETING DATE

Wednesday, June 27, 2018 at 6:00pm at the Central Fire Station

15. ADJOURNMENT

The meeting adjourned without objection at 7:31 pm. Kellie Johnson, Vice-Chairperson



CHARTER TOWNSHIP OF MERIDIAN Development Projects Update

July 2018

Businesses Opened:

- **Baskin-Robbins**, 1917 Grand River Ave – Open House 6.2.18
- **Elevation**, Jolly & Jolly Oak Roads, 350+ residential units, 6.19.18 is opening party
- **Great Lakes Interiors**, office furniture retailer at 2076 Towner Road
- **Experimac**, Apple/Mac sales, trades and repairs at 4790 S. Hagadorn/Hannah Plaza
- **Mannik & Smith**, public & private consultants at 2193 Association Drive Okemos, open house 6.28.2018
- **Aldi**, renovation complete, reopened now

Ribbon Cuttings/Opening Dates Announced (some estimated):

- **Launch Trampoline Park**, 30k sq. ft. in Meridian Mall. Plan to open August/Sept.

Under construction

- **Elevation**, Jolly & Jolly Oak Roads, 350+ residential units
- **Peak Performance Physical Therapy**, new Meridian location next to Playmakers
- **Soldans Pet Supply**, 2283 W. Grand River Ave. Relocation expected in December 2018
- **Red Cedar Flats**, 112 unit, mixed use project with 11,800 square feet of commercial space on Northwind Drive.
- **Chamberlin Townhouses**, Marsh Road west of Lake Lansing – Townhomes under construction
- **Portnoy and Tu dentist office**, 4,332 square foot dental office at 2476 Jolly Road.
- **Ingham County Medical Care Facility**, 64,000 square foot (48 room) addition at 3860 Dobie.
- **Marriot Courtyard**, 105 rooms at SW corner of Jolly Oak Road/Meridian Crossing Drive.
- **Best Buy**, Shopping Center – exterior facelift, resizing spaces. Valued at \$770,000

Approved/not yet commenced

- **Tilted Kilt**, new restaurant at 5000 Northwind Drive (Red Cedar Flats)
- **Silverstone Estates**, 25 single family residential homes at Powell Road north of Grand River

Under consideration

- **Red Cedar Manor**, 2875 Northwind Drive. 70,000+ sq ft mixed use w/88 apartments
- **Panera Bread**, Proposing to demo and rebuild BD Mongolian Grill. Drive-through proposed.
- **New Hope Church**, Saginaw Highway & Newton Roads, construction expected in summer
- **Hannah Farms East**, mixed use/student housing proposed at Eyde Pkwy (dormant for now)
- **Copper Creek Condominiums**, 45 acres NE corner of Haslett & Van Atta. 102 residential lots.

Renovations

- **Olive Garden**, Exterior remodel, complete interior makeover. No value yet

Closings/Relocations

- **Toys R Us**, Property up for auction
- **Younkers**, closing both anchor stores at Meridian Mall by August
- **Gilbert & Blakes**, sold and rebranding to Lucky's Steakhouse in August
- **Leo's Pub & Grub**, lease not renewed
- **Penn Station, Marsh & Grand River**, closed Okemos Location after 5 years



To: Corridor Improvement Authority

From: Peter Menser, Principal Planner
Justin Quagliata, Assistant Planner

Date: June 22, 2018

Re: Commercial Planned Unit Development #18024 (Affinity 9 Investments, LLC), demolish existing restaurant and construct a new restaurant with drive-through window at 2080 Grand River Avenue.

Jeffrey W. Kyes of Kebs, Inc., on behalf of Affinity 9 Investments, LLC, has submitted a request to establish a commercial planned unit development (C-PUD) at 2080 Grand River Avenue. The 1.16 acre subject site is zoned C-2 (Commercial). The redevelopment plans include the demolition of the existing Mongolian BBQ restaurant and the construction of a new approximately 4,534 square foot Panera restaurant with drive-through window. A portion of the project site is located in the floodplain. The Township requires special use permits for a drive-through window and work in the floodplain. However, a C-PUD serves as the special use permit review for any case requiring a special use permit. The Planning Commission reviewed a C-PUD concept plan for the project at its meeting on January 8, 2018.

The Future Land Use Map from the 2017 Master Plan designates the subject property in the Mixed Use Core category.

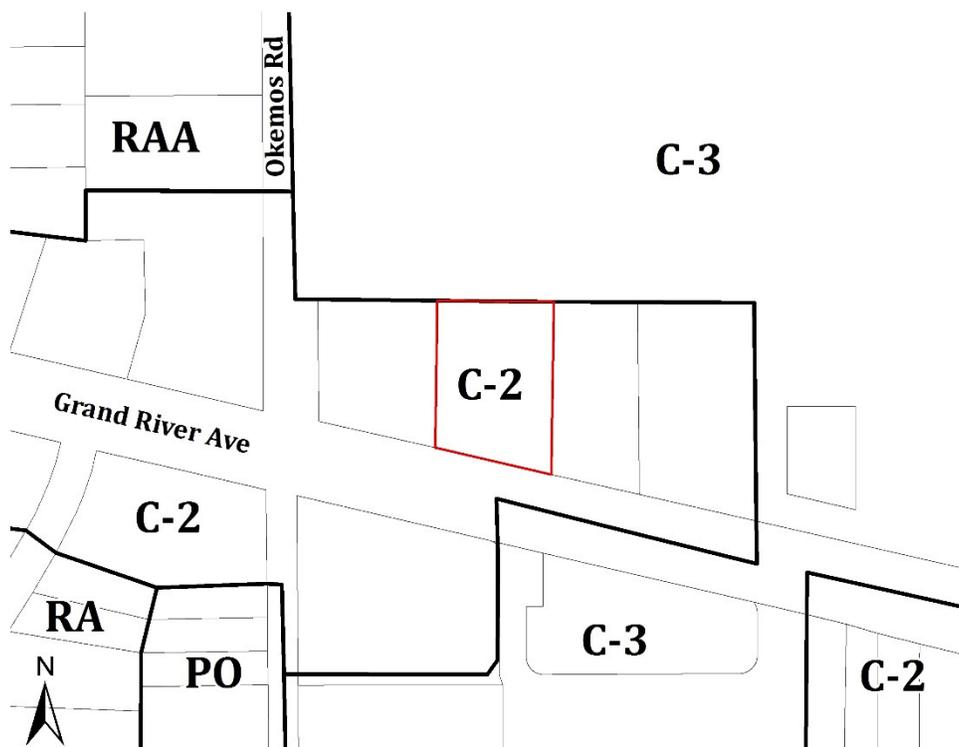
FUTURE LAND USE MAP



Zoning

The subject site is zoned C-2 (Commercial). With 200.51 feet of frontage on Grand River Avenue and 1.16 acres of lot area, the site meets and exceeds the minimum requirements of 100 feet of lot frontage and 4,000 square feet of lot area for the C-2 zoning district. A C-PUD is permitted in the C-1, C-2, C-3 (Commercial), and CS (Community Service) zoning districts. All uses permitted by right and by special use permit in the underlying C-2 zoning district are permitted in a C-PUD.

ZONING MAP



Physical Features

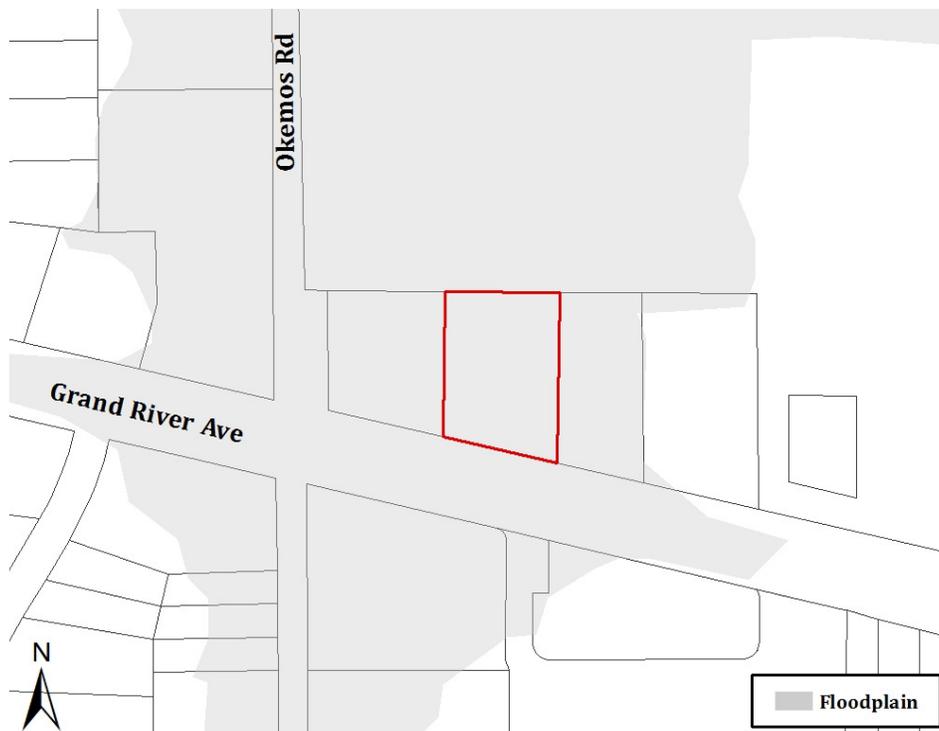
The site is currently developed with a vacant 4,929 square foot restaurant building, a paved parking lot, shrubs and bushes, and an associated access drive. Landscaping consisting of grass, shrubs, and trees are installed around the building and in the parking lot. The site does not have any special designation on the Township Greenspace Plan.

Floodplain

A portion of the project site is located in the floodplain. The submitted site plan depicts the elevation of the floodplain at 842.1 feet above mean sea level; the lowest floor of the new building will be located one foot above the floodplain. The applicant is proposing to place 150 cubic yards of fill in the floodplain to facilitate re-grading of the parking lot to eliminate parking and sidewalk grades that are currently over 5% and reduce parking lot runoff to Grand River Avenue. A 410 cubic yard storm water detention area is proposed to the west of the site entrance, and also serves as compensating excavation for fill in the floodplain.

The Michigan Department of Environmental Quality (MDEQ) reviewed the proposed work in the floodplain and issued approval on May 3, 2018. The Township requires a special use permit for work in the floodplain; however, a C-PUD serves as the special use permit review for any case requiring a special use permit, including work in the floodplain.

Floodplain Map



Wetlands

Township Wetland #21-3 is located to the north of the subject site. A delineation was conducted by the Township's Environmental Consultant to determine the boundary, regulatory status, and size of the wetland. Section 86-471 of the Code of Ordinances requires that all structures and grading activities be set back 40 feet from a delineated wetland boundary and that a natural vegetation strip be maintained within 20 feet of the wetland boundary.

Wetland Map



Streets and Traffic

Access to the site will be from Grand River Avenue, which is classified as a Principal Arterial on the Street Setbacks and Service Drives Map (Section 86-367). Grand River Avenue is a four lane road with curb and gutter and a center turn lane. A seven foot wide pedestrian-bicycle pathway is installed along the frontage of the site. The most recent (2016) traffic count information from the Michigan Department of Transportation (MDOT) for Grand River Avenue, between Okemos Road and Marsh Road, showed a total of 23,554 vehicles in a 24 hour period.

A traffic impact study is required for new special uses which would generate over 100 directional trips during a peak hour of traffic, or over 750 trips on an average day. The applicant submitted a traffic impact study prepared by Traffic Engineering Associates, Inc. dated December 2017 that provides information on traffic generated by the proposed C-PUD. The study looks at existing, background (future traffic volumes without the traffic generated by the proposed development), and future level of service (LOS) during the AM (7:30-8:30 a.m.) and PM (5:00-6:00 p.m.) peak hours at the following three intersections around the project site:

- Grand River Avenue and Okemos Road
- Grand River Avenue and Marsh Road
- Grand River Avenue and Meijer/Chick-Fil-A driveway

The traffic study notes that existing traffic at the studied intersections all operate at an acceptable LOS (LOS D or better) during the AM and PM peak hours, with the exception of several movements at the intersection of Grand River Avenue and Okemos Road, and at the Grand River Avenue and Meijer/Chick-Fil-A driveway. The study shows that background traffic at the studied intersections will operate at an acceptable level of service during AM and PM peak hours, with the exception of several movements at the intersection of Grand River Avenue and Okemos Road, and at the Grand River Avenue and Meijer/Chick-Fil-A driveway.

For future traffic, the study indicates that all studied intersections will continue to operate at an acceptable LOS during the AM and PM peak hours, with the exception of several movements at the following intersections:

Grand River Avenue and Okemos Road

- During the AM peak hour, the westbound through-right movement on Grand River Avenue is expected to operate at a LOS E with 70.3 seconds of vehicle delay, an increase of 7.3 seconds from background conditions.
- During the AM peak hour, the northbound left turn movement on Okemos Road to westbound Grand River Avenue is projected to operate at a LOS F with 83.4 seconds of vehicle delay.
- During the PM peak hour, the eastbound through movement on Grand River Avenue is anticipated to operate at a LOS F with 75.5 seconds of vehicle delay, an increase of 5.1 seconds from background conditions.
- During the PM peak hour, the northbound left turn movement on Okemos Road to westbound Grand River Avenue is expected to operate at a LOS E with 65.0 seconds of vehicle delay.
- During the PM peak hour, the northbound through-right movement on Okemos Road is projected to operate at a LOS E with 60.8 seconds of vehicle delay.

- During the PM peak hour, the southbound left turn movement on Okemos Road to eastbound Grand River Avenue is anticipated to operate at a LOS E with 63.3 seconds of vehicle delay, an increase of 0.5 seconds from background conditions.

Grand River Avenue and Marsh Road

- During the AM peak hour, the westbound through movement on Grand River Avenue is projected to operate at a LOS E with 61.2 seconds of vehicle delay, an increase of 6.6 seconds from background conditions.
- During the PM peak hour, the eastbound through-right movement on Grand River Avenue is anticipated to operate at a LOS E with 55.6 seconds of vehicle delay, an increase of 1.0 second from background conditions.
- During the PM peak hour, the southbound left turn movement on Marsh Road to eastbound Grand River Avenue is expected to operate at a LOS E with 64.6 seconds of vehicle delay.

Grand River Avenue and Meijer/Chick-Fil-A driveway

- During the PM peak hour, the northbound left turn movement from the Meijer/Chick-Fil-A driveway to westbound Grand River Avenue is projected to operate at a LOS F with 80.9 seconds of vehicle delay, an increase of 2.5 seconds from background conditions.

The traffic impact study estimated traffic generation for the proposed project based on a 4,375 square foot *Fast-Food Restaurant with Drive-Through Window*, which is Land Use Code 934 in the Institute of Transportation Engineers (ITE) Trip Generation Manual. Land Use Code 934 describes a restaurant characterized by a large carry-out clientele, long hours of service, and high turnover rates for eat-in customers. The following table estimates traffic generation for the proposed project.

	Proposed C-PUD
Peak Hour trips	176 (a.m.) 143 (p.m.)
Total Weekday Traffic	2,060 trips

The applicant’s traffic consultant recommends that a three lane site entrance/exit be designed for the site driveway on Grand River Avenue to provide one exclusive exiting left turn lane, one exiting right turn lane, and one entering lane. Additionally, MDOT guidelines show that there is justification for a right turn lane for westbound Grand River Avenue at the site driveway.

Utilities

The existing building is served by municipal water and sanitary sewer, both of which are available to serve the proposed redevelopment. The location and capacity of utilities will be reviewed in detail during site plan review if the C-PUD is approved.

Staff Analysis

The applicant is proposing to develop the site using the commercial planned unit development (C-PUD) ordinance. A C-PUD is reviewed in accordance with the provisions of Section 86-444 of the Code of Ordinances. A C-PUD serves as the special use permit review for any case requiring a special use permit, therefore separate approvals are not required. For this project the special use permits that would normally be required for a drive-through window and work in the floodplain will be covered under the C-PUD review. A special use permit is reviewed in accordance with the provisions of Section 86-126 of the Code of Ordinances. Section 86-436(n) of the Code of Ordinances provides additional standards for projects in the floodplain, which are as follows:

1. All new residential structures and residential structures requiring substantial improvements shall have the lowest floor, including basement, elevated to one foot above the level of the base flood elevation.
2. All new nonresidential structures and nonresidential structures requiring substantial improvements shall have the lowest floor, including basement, elevated to one foot above the level of the base flood elevation or shall be floodproofed to one foot above the level of the base flood elevation.
3. Excavation and shaping of the floodway fringe shall be conducted in such a manner as to maintain or improve the natural impoundment capacity of the base flood elevation. In no case shall the impoundment capacity of the floodway fringe be reduced.

The lowest floor of the new building will be located one foot above the floodplain elevation of 842.1. The placement of 150 cubic yards of fill in the floodplain when considered along with the compensating cut of 410 cubic yards for the storm water detention area, will not reduce the impoundment capacity of the floodplain.

The Code of Ordinances standards for a drive-through include provisions for an order window, stacking for three cars, and a bypass lane. The submitted site plan indicates the project meets the ordinance standards for a drive-through window.

The C-PUD process mirrors that of the mixed use planned unit development (MUPUD) process, with public hearings held by both the Planning Commission and Township Board. The Planning Commission makes a recommendation on the project with the Township Board having final approval.

Following is a summary of the project's consistency with the C-PUD ordinance standards. As part of this project analysis, standards for the underlying C-2 zoning district and other requirements applicable to development in the C-2 district will be compared to what is proposed on the submitted site plan.

Land use: All uses permitted by right and by special use permit in the underlying C-2 zoning district are permitted in a C-PUD. The proposed land use is a restaurant with drive-through window.

Performance objectives: Minimum C-PUD performance objectives include the following:

- *Provide good internal and external access to the street for pedestrians and bicycles* – a painted cross walk is proposed to connect pedestrians and bicyclists to the building from the pathway along Grand River Avenue to the restaurant.
- *Minimize environmental impacts by using green building and site development standards* – the submitted site plan does not indicate whether environmental impacts will be minimized.
- *Enhance access to alternative transportation modes including public transportation* – the subject site is located along CATA route 1.
- *Use of quality building materials* – generally, building materials in a C-PUD should include, but are not limited to, wood, brick, clapboards, beadboard, glass, and stone. Other materials such as vinyl, aluminum, and other metal sidings should be avoided. All buildings should be completed on all sides with acceptable materials. The design of the building should relate to and blend with the facades of adjacent buildings and complement streetscape improvements in the area. The proposed building material for the restaurant is a brick masonry system. A sample board of the building materials submitted by the applicant will be displayed at the public hearing.
- *Provide for buffering between any conflicting feature of the design and an adjacent residential land use* – the site is surrounded entirely by commercial land uses.
- *Adherence to smart growth principles* – the proposed project would redevelop a previously developed site and is located within the Urban Services Boundary.
- *Preference for parking located in the rear or side yard* – the setbacks for a parking area and the number of parking spaces required can be waived in a C-PUD. 47 parking spaces are required for the project and the applicant is proposing 52 parking spaces. Parking is located in the front, rear, and side yards.

Railings, benches, trash receptacles, or bicycle racks: Accessory items such as railings, benches, trash receptacles, or bicycle racks shall be of commercial quality and complement the building design and style. Three bicycle racks (six bicycle parking spaces) are shown on the submitted site plans. Information on proposed railings, benches, trash receptacles, or other accessory items were not provided.

The following section summarizes other aspects of the project:

Bicycle parking: One bicycle parking space is required for every ten motor vehicle parking spaces required. There are 47 parking spaces required; therefore the project must have a minimum of five bicycle parking spaces. Three bicycle parking racks (six bicycle parking spaces) are shown on the submitted site plan near the main entry to the building.

Landscaping: Proposed landscaping must generally comply with the provisions of the Code of Ordinances. Section 86-758 of the Code of Ordinances outlines the typical landscape requirements for off-street parking areas including landscaped islands at least ten feet in width, building perimeter landscaping at least four feet in width, a minimum of 200 square feet of interior landscaping for every ten parking spaces, and two interior canopy trees per ten parking spaces. Section 86-473 provides standards for street trees. A landscape plan was provided and notes building perimeter landscaping is to be determined.

Outdoor lighting: The C-PUD ordinance requires site lighting to comply with the Outdoor Lighting Ordinance (Section 38-371). A photometric plan was submitted and will be reviewed in detail during site plan review if the C-PUD is approved.

Waiver requests

The C-PUD ordinance is intended to provide flexibility for the Planning Commission and Township Board to set appropriate standards during the review process. The C-PUD ordinance generally waives the standard requirements for lot size, setbacks, number of required parking spaces, building perimeter landscaping, and impervious surface coverage, provided the purpose and intent of the ordinance is incorporated into the overall development plan. Based on the submitted site plan the applicant is requesting the following waivers for the project:

Impervious surface coverage

The maximum impervious surface allowed in the C-2 zoning district is 70 percent. The project proposes 74.10 percent. A waiver of 4.10% is requested.

Parking lot landscape buffer (side and rear)

A 15 foot wide landscaped buffer is required between a parking area and a property line. The submitted site plan shows landscape buffers of 13.14 feet, 3.79 feet, and 4.00 feet from the north, east, and west property lines and is therefore requesting waivers of 6.86 feet, 11.21 feet, and 11.00 feet.

Wetland setback

All structures and grading activities are required to be set back 40 feet from regulated wetlands equal to or greater than two acres in area. The applicant is proposing an 11.63 foot parking setback from the north property line. A waiver of 28.37 feet is requested.

Natural vegetation strip

A 20 foot natural vegetation strip must be maintained from the edge of a regulated wetland. The applicant is proposing a six foot grading setback. A waiver of 14 feet is requested.

Amenities

One or more amenities are required for every C-PUD project. Examples of amenities are grouped into categories in the zoning ordinance, which include conservation, environment, accessibility, parks, recreation and culture, social interaction, and site and building design. If multiple amenities are shown, the ordinance suggests they should represent multiple categories. Criteria to determine whether a proposed amenity is acceptable for consideration are found in Section 86-444(e)(2) of the C-PUD ordinance.

Sheet 1 of the submitted site plan shows the proposed amenities for the project, including connection to the pedestrian-bicycle pathway along Grand River Avenue and public Wi-Fi.

Attachment

1. Site plan dated April 16, 2018, received by the Township on April 17, 2018

G:\Community Planning & Development\Planning\COMMERCIAL PLANNED UNIT DEVELOPMENT (CPUD)\2018\CPUD 18024 (Affinity 9 Investments)\CPUD 18024.cia1.docx

10/24/2017 11:52:50 AM



1 | PERSPECTIVE VIEW



2 | PERSPECTIVE VIEW



3 | PERSPECTIVE VIEW

Bakery-Cafe:

#0715

Project Team:



Professional Seal:

Project Title:

Bakery Cafe #0715
2080 GRAND RIVER AVE
OKEMOS, MI 48864

PERMIT / BID SET



Consultant Copyright Placeholder

No.	Description	Date

EXTERIOR PERSPECTIVES

Project Number: _____ Sheet Number: _____

170686

Drawn By: _____

ARC.V.MH

Issue Date: _____

10.24.17

DPM: _____

BRIAN B. _____

DM: _____

DAN C. _____

CPM: _____

KEVIN C. _____

A300

P8 2015.01



Bakery Cafe #0715
2080 GRAND RIVER AVE
OKEMOS, MI 48864



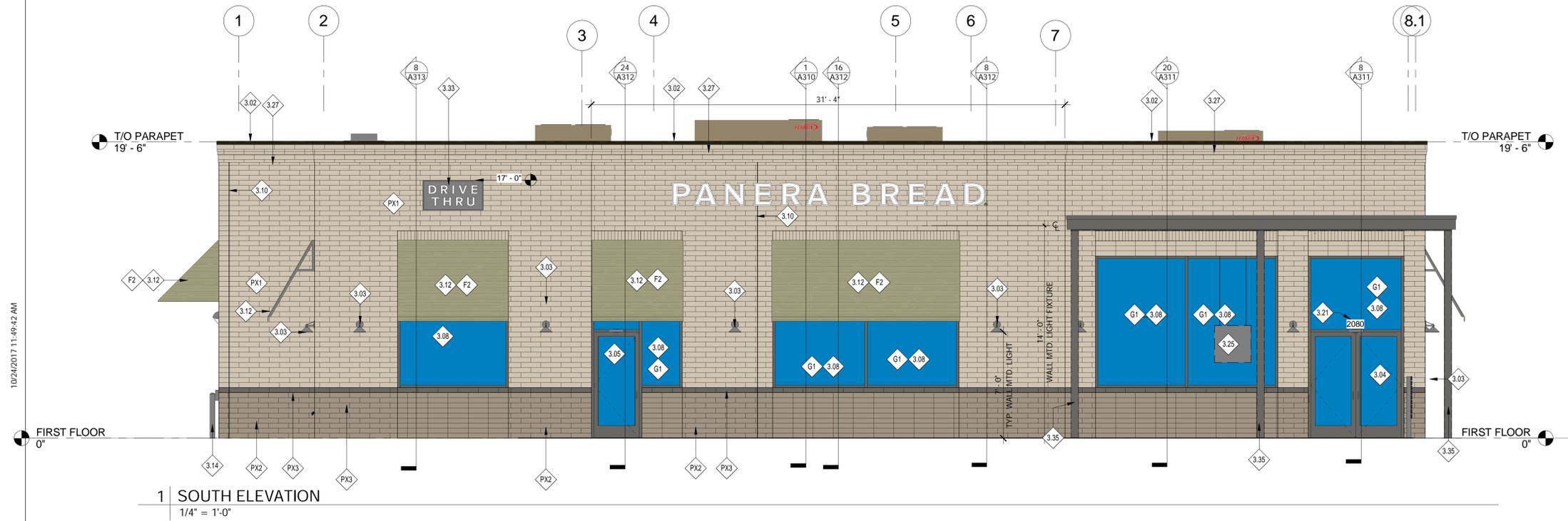
Consultant Copyright Placeholder

GENERAL NOTES:

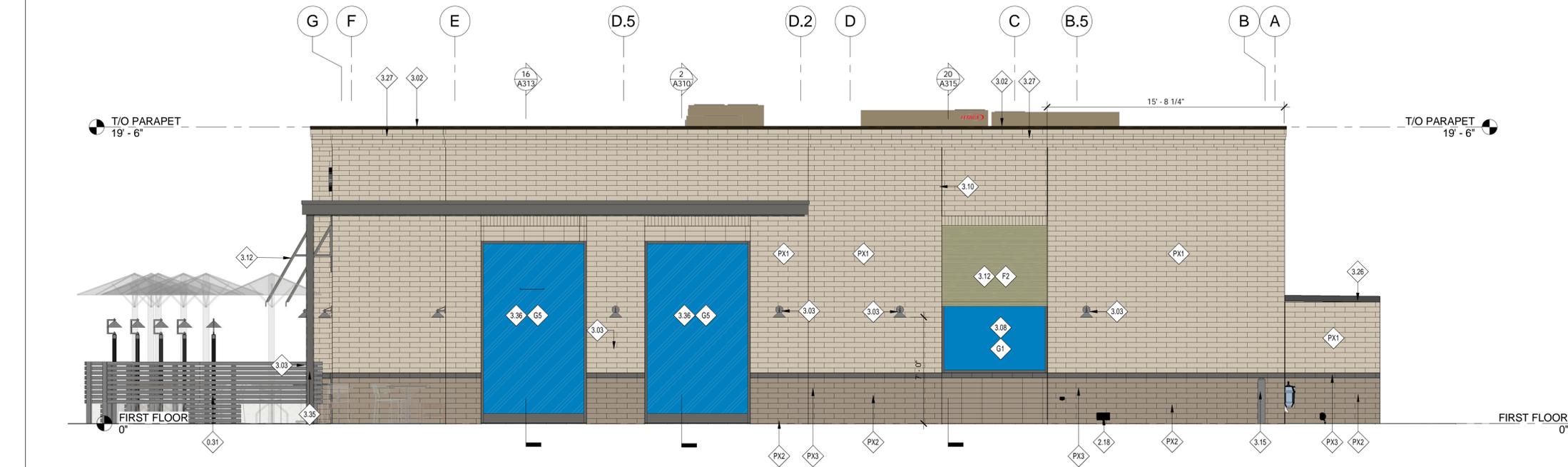
A. ALL GLAZING TO BE G1 UNLESS NOTED OTHERWISE. FOR STOREFRONT / WINDOW ELEVATIONS SEE SHEET A611.

KEYED NOTES

- 0.31 PATIO SIDEWALK LIGHT BY PANERA. REFER TO TENANT REFLECTED CEILING PLAN FOR FURTHER INFORMATION.
- 2.18 NEW EXTERIOR GROUND LIGHT
- 3.02 PRE-FINISHED METAL COPING | FINISH: TO MATCH HC-166 KENDALL CHARCOAL
- 3.03 WALL MOUNTED LIGHT FIXTURE. FIXTURES SHALL BE PROVIDED AND INSTALLED BY GC. GC TO PROVIDE POWER AS REQUIRED. REFER TO REFLECTED CEILING PLAN FOR ADDITIONAL INFORMATION.
- 3.04 MAIN ENTRANCE/EXIT | REFER TO A601 AND A611 FOR ADDITIONAL INFORMATION.
- 3.05 SECONDARY ENTRANCE/EXIT | REFER TO A601 AND A611 FOR ADDITIONAL INFORMATION.
- 3.08 ALUMINUM STOREFRONT SYSTEM WITH 1" INSULATED GLAZING. COLOR: TO MATCH HC-166 KENDALL CHARCOAL REFER TO SHEET A611 FOR FURTHER INFORMATION.
- 3.10 CONTROL JOINT(S) WHERE INDICATED | PROVIDE SEALANT TO MATCH ADJACENT WALL MATERIAL.
- 3.12 PRE-MANUFACTURED FABRIC AWNING PROVIDED AND INSTALLED BY SIGN VENDOR UNDER SEPARATE PERMIT. GC SHALL PROVIDE APPROPRIATE BLOCKING AS REQUIRED BY SIGN VENDOR. REFER TO SHOP DRAWINGS FOR ADDITIONAL INFORMATION.
- 3.14 4" DIA. CONCRETE BOLLARD | FINISH: PAINT TO MATCH P107 | REFER TO SHEET A014 FOR FURTHER INFORMATION
- 3.15 6" DIA. CONCRETE BOLLARD | FINISH: PAINT P107 | REFER TO SHEET A014 FOR FURTHER INFORMATION
- 3.21 BUILDING ADDRESS SIGN | VERIFY SIZE, LOCATION, AND STYLE WITH LOCAL FIRE DEPARTMENT
- 3.25 PANERA LOGO SIGN. SEE SIGN SHOPS FOR MORE INFORMATION. PROVIDE BLOCKING AS REQUIRED.
- 3.26 STANDING SEAM METAL ROOFING SYSTEM AND EDGE PIECE BY MFR. 12" SPACING. | COLOR: TO MATCH HC-166 KENDALL CHARCOAL
- 3.27 BRICK CORNICE TO BE CONSTRUCTED USING ENDURAMAX BRICK WALL SYSTEM. REFER TO DETAIL FOR FURTHER INFORMATION.
- 3.33 INTERNALLY ILLUMINATED LED 'DRIVE THRU' SIGN BY TENANT. COLOR: BLACK CHANNEL WITH REVERSE (HALO) ILLUMINATION AND WHITE LETTERS. ALL SIGNAGE UNDER SEPARATE PERMIT. G.C. SHALL PROVIDE BLOCKING IN WALL FOR SIGN AND PROVIDE ACCESS PANEL ON BACKSIDE OF PARAPET FOR ACCESS AS REQUIRED.
- 3.35 PREFABRICATED ALUMINUM ENTRY CANOPY CONSTRUCTION. ALL EXPOSED ALUMINUM TO BE FINISHED TO MATCH HC-166 KENDALL CHARCOAL.
- 3.36 ALUMINUM STOREFRONT SYSTEM WITH 1-1/8" INSULATED GLAZING. COLOR: TO MATCH HC-166 KENDALL CHARCOAL REFER TO SHEET A611 FOR FURTHER INFORMATION
- F2 FABRIC AWNING | OPEN ENDED SLAB | COLOR: ASPEN FABRIC. (4' PROJECTION / 5' HEIGHT AT TYPICAL & 5' PROJECTION @ DRIVE THRU)
- G1 ALUMINUM STOREFRONT | 2" x 4 1/2" PROFILE | THERMAL BREAK | COLOR: #HC-166 KENDALL CHARCOAL | 1" CLEAR INSULATED GLAZING: PPG SOLARBAN 70XL (OR APPROVED EQUAL) | REFER TO SHEET A611 FOR ADDITIONAL INFORMATION
- G5 ALUMINUM STOREFRONT | 2" x 6" PROFILE | THERMAL BREAK | COLOR: #HC-166 KENDALL CHARCOAL | 1-1/8" CLEAR INSULATED GLAZING: PPG SOLARBAN 70XL (OR APPROVED EQUAL) | REFER TO SHEET A611 FOR ADDITIONAL INFORMATION
- PX1 PAINT SYSTEM OVER BRICK MASONRY SYSTEM | MFR: ENDURAMAX, CLAY BRICK, MODULAR | COLOR: #955 BERBER WHITE
- PX2 PAINT SYSTEM OVER BRICK MASONRY SYSTEM | MFR: ENDURAMAX, CLAY BRICK, MODULAR | COLOR: #AF-155 WEINMARANER
- PX3 PAINT SYSTEM OVER BRICK MASONRY SYSTEM | MFR: ENDURAMAX, CLAY BRICK, MODULAR | COLOR: #HC-166 KENDALL CHARCOAL



1 | SOUTH ELEVATION
 1/4" = 1'-0"



2 | EAST ELEVATION
 1/4" = 1'-0"

No.	Description	Date

EXTERIOR ELEVATIONS

Project Number: 170686
 Drawn By: CAO_MH
 Issue Date: 10.24.17
 DPM: BRIAN B. DM: DAN C. CPM: KEVIN C.
A301

10/24/2017 11:49:42 AM

PB 2015.01



Bakery Cafe #0715
2080 GRAND RIVER AVE
OKEMOS, MI 48864

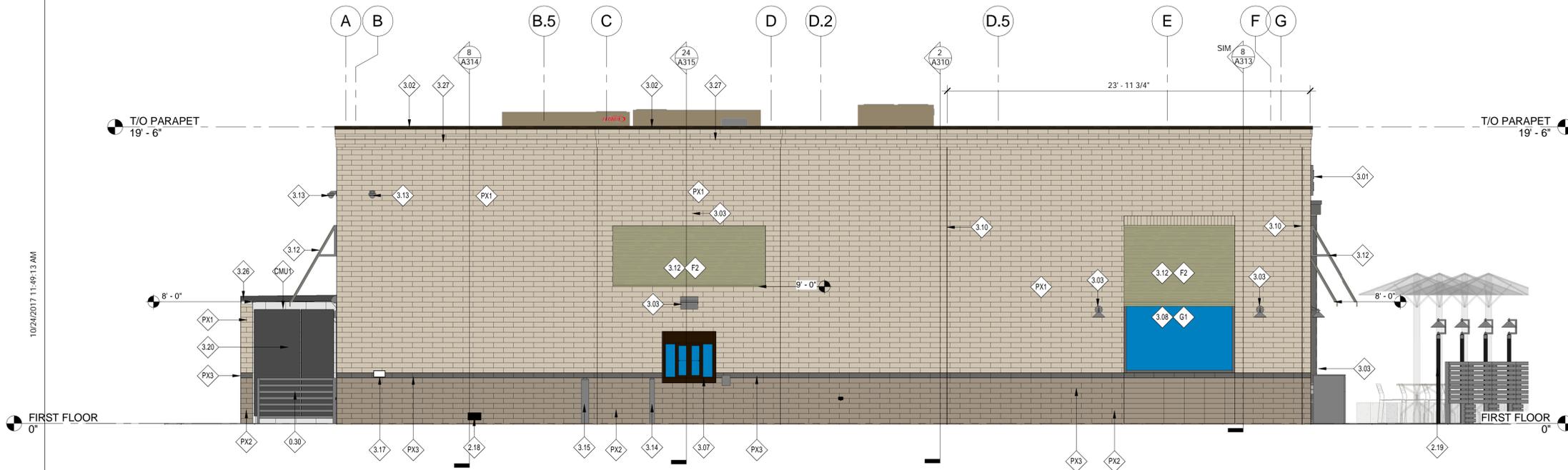


GENERAL NOTES:

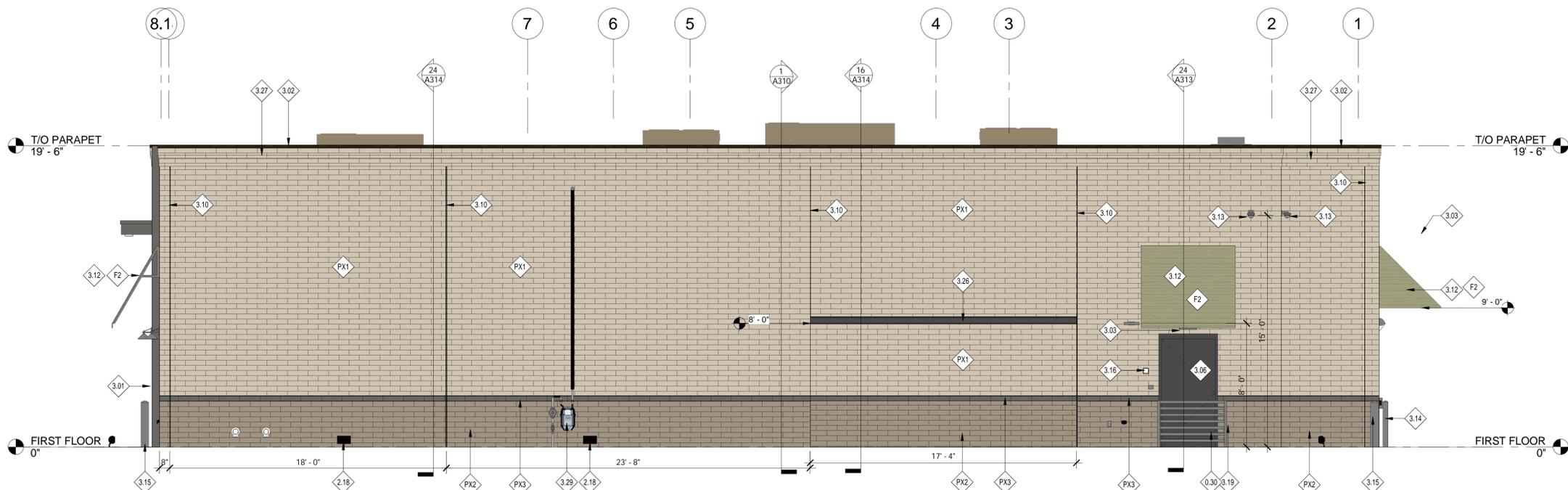
A. ALL GLAZING TO BE G1 UNLESS NOTED OTHERWISE. FOR STOREFRONT / WINDOW ELEVATIONS SEE SHEET A611.

KEYED NOTES

- 3.00 PATIO RAILING BY PANERA. COLOR: TO MATCH #AF-155 WEIMARANER. REFER TO 13/A014. BY TENANT
- 2.18 NEW EXTERIOR GROUND LIGHT
- 2.19 SIDEWALK POLE LIGHT FIXTURE
- 3.01 INTERNALLY ILLUMINATED CLIP LETTERS PROVIDED AND INSTALLED BY SIGN VENDOR UNDER SEPARATE PERMIT. G.C. TO PROVIDE BLOCKING IN WALL FOR SIGN AND PROVIDE ACCESS PANEL ON BACKSIDE OF PARAPET FOR ACCESS AS REQUIRED. (UNDER SEPARATE SIGNAGE PERMIT)
- 3.02 PRE-FINISHED METAL COPING | FINISH: TO MATCH HC-166 KENDALL CHARCOAL
- 3.03 WALL MOUNTED LIGHT FIXTURE. FIXTURES SHALL BE PROVIDED AND INSTALLED BY GC. GC TO PROVIDE POWER AS REQUIRED. REFER TO REFLECTED CEILING PLAN FOR ADDITIONAL INFORMATION.
- 3.06 HOLLOW METAL SERVICE DOOR. REFER TO A601 FOR ADDITIONAL INFORMATION. | PAINT TO MATCH HAC-166 KENDALL CHARCOAL
- 3.07 DRIVE-THRU WINDOW | MFR: QUIKSERV | MODEL: FM42E | COLOR TO MATCH STOREFRONT.
- 3.08 ALUMINUM STOREFRONT SYSTEM WITH 1" INSULATED GLAZING. COLOR: TO MATCH HC-166 KENDALL CHARCOAL REFER TO SHEET A611 FOR FURTHER INFORMATION.
- 3.10 CONTROL JOINT(S) WHERE INDICATED | PROVIDE SEALANT TO MATCH ADJACENT WALL MATERIAL
- 3.12 PRE-MANUFACTURED FABRIC AWNING PROVIDED AND INSTALLED BY SIGN VENDOR UNDER SEPARATE PERMIT. GC SHALL PROVIDE APPROPRIATE BLOCKING AS REQUIRED BY SIGN VENDOR. REFER TO SHOP DRAWINGS FOR ADDITIONAL INFORMATION.
- 3.13 SECURITY CAMERA INSTALLED BY TENANT | COORDINATE ROUGH-IN LOCATION(S) AND HEIGHTS WITH PANERA CONSTRUCTION MANAGER PRIOR TO INSTALLATION.
- 3.14 4" DIA. CONCRETE BOLLARD | FINISH: PAINT TO MATCH P107 | REFER TO SHEET A014 FOR FURTHER INFORMATION
- 3.15 6" DIA. CONCRETE BOLLARD | FINISH: PAINT P107 | REFER TO SHEET A014 FOR FURTHER INFORMATION
- 3.16 FIRE DEPARTMENT ACCESS BOX | INSTALLED ADJACENT SERVICE DOOR. COORDINATE LOCATION WITH LOCAL FIRE DEPARTMENT.
- 3.17 FIRE DEPARTMENT CONNECTION (FDC) BY LANDLORD
- 3.19 HOSE BIBB | RECESSED STAINLESS STEEL BOX. REFER TO PLUMBING DRAWINGS FOR LOCATION AND ADDITIONAL INFORMATION. INSTALLED BY TENANT.
- 3.20 GATE ENCLOSURE AT CART CORRAL | MFR: TREX COMPOSITE PLANK | COLOR: SADDLE.
- 3.26 STANDING SEAM METAL ROOFING SYSTEM AND EDGE PIECE BY MFR. 12" SPACING. | COLOR: TO MATCH HC-166 KENDALL CHARCOAL
- 3.27 BRICK CORNICE TO BE CONSTRUCTED USING ENDUROMAX BRICK WALL SYSTEM. REFER TO DETAIL FOR FURTHER INFORMATION.
- 3.29 GAS METER - REFER TO PLUMBING DRAWINGS FOR FURTHER INFORMATION
- CMU1 SMOOTH FACE CONCRETE BLOCK. PAINT TO MATCH MAIN BUILDING
- F2 FABRIC AWNING | OPEN ENDED SLAB | COLOR: ASPEN FABRIC, (4' PROJECTION / 5' HEIGHT AT TYPICAL & 5' PROJECTION @ DRIVE THRU)
- G1 ALUMINUM STOREFRONT | 2" x 4 1/2" PROFILE | THERMAL BREAK | COLOR: #HC-166 KENDALL CHARCOAL | 1" CLEAR INSULATED GLAZING: PPG SOLARBAN 70XL (OR APPROVED EQUAL) | REFER TO SHEET A611 FOR ADDITIONAL INFORMATION
- PX1 PAINT SYSTEM OVER BRICK MASONRY SYSTEM | MFR: ENDURAMAX, CLAY BRICK, MODULAR | COLOR: #955 BERBER WHITE
- PX2 PAINT SYSTEM OVER BRICK MASONRY SYSTEM | MFR: ENDURAMAX, CLAY BRICK, MODULAR | COLOR: #AF-155 WEIMARANER
- PX3 PAINT SYSTEM OVER BRICK MASONRY SYSTEM | MFR: ENDURAMAX, CLAY BRICK, MODULAR | COLOR: #HC-166 KENDALL CHARCOAL



1 WEST ELEVATION
1/4" = 1'-0"



2 NORTH ELEVATION
1/4" = 1'-0"

No.	Description	Date

EXTERIOR ELEVATIONS

Project Number: 170686
 Drawn By: CAO_MH
 Issue Date: 10/24/17
 DPM: BRIAN B. DM: DAN C. CPM: KEVIN C.

A302



8.A

To: Corridor Improvement Authority

From: Chris Buck, Economic Development Director

Date: July 25, 2018

Re: Corridor Plan – Goals, Objectives, Timelines, Responsible Parties, Budget

Since we did not have our scheduled meeting in June, I wanted to update the CIA board on what has transpired since May. The Redevelopment Ready Communities (RRC) certification we are seeking from the Michigan Economic Development Corporation (MEDC) requires the Township to have an adopted Corridor Plan. In my months-long communication with my MEDC contact, they are well aware of our Master Plan, creation of the CIA and the subsequent work the Township has been doing with the Shaping the Avenue consulting team and eventual adoption of Form Based Code.

Their recommendation, to satisfy the Corridor Plan criteria, is to have the Township Board adopt the Power Point presentation the Shaping the Avenue team presented during the Planning Commission work session in April 2018 as our Corridor Plan on record. It captures the path the Township is following, it mentions the Capitol Corridor book from 2014 as the regional plan for the corridor, and refers to the drafted Form Based Code we are still reviewing and revision before eventual adoption. The Township Board did adopt this as our Corridor Plan on July 10, 2018.

The final piece is to create the goals and objectives, timelines, responsible parties and budget (if any) – essentially the Implementation Plan for the Corridor Plan. I drafted goals & objectives for your review and discussion at this upcoming meeting. This draft includes the steps we took to create the CIA and the process the Township is in the middle of regarding adopting form based code. The rest I took from the CIA's Inaugural Work Plan you created with Ben Motil before I was involved with the CIA. Since it was created by this board, it seems logical that we should create the actual action steps based on those goals and objectives from the work plan.

Please review this work plan, the Power Point from Shaping the Avenue team and my draft grid and let's have a great discussion outlining the action steps we need to take as a CIA board and staff to move the Corridor Plan forward.

Attachments

1. CIA Inaugural Work Plan
2. Shaping the Ave Corridor Plan – April 2018
3. Corridor Plan – Implementation Grid

MERIDIAN TOWNSHIP
CORRIDOR IMPROVEMENT AUTHORITY
WORK PLAN

GOAL 1: Promote a consistent and pleasing aesthetic style for buildings, grounds, and public right-of-way.

- **Objective:** Identify and coordinate streetscape improvements needed throughout the corridor to achieve the desired traditional look for Township. This will include items such as brick pavers, street trees, lights, benches, waste receptacles, etc.

GOAL 2: Provide a common vision for corridor businesses.

- **Objective:** This will be accomplished through SWOT analysis and roundtable discussions in the community. Data will be collected and compiled to gather community vision for the corridor. By gaining better insight of how the collective, “common vision” is defined in the community, the CIA can provide a better framework of design for corridor businesses.

GOAL 3: Strive to increase safety and accessibility for pedestrians & cyclists.

- **Objective:** This will be accomplished through creation of a path and sidewalk system that provides safe refuge for non-motorists while traveling along the corridor and between buildings.
- **Objective:** Review the Township pathway plan that identifies the most appropriate treatment for each section of the corridor. This pathway plan will include more precise cost estimates so the Township can seek grants and alternate ways to assist property owners.

GOAL 4: Promote smart growth on the corridor and include infill development while discouraging sprawl.

- **Objective:** Create a long-range development strategy that focuses on solid growth, expanded employment and stable tax revenues.
- **Objective:** Institute an early warning system for business closings, relocations and layoffs. Objective: Identify corridor companies ripe for expansion. Remove barriers to their growth.

GOAL 5: Improve recognition of corridor entry ways and signage.

- **Objective:** Designate attractive corridor entryways with landscaping, signage, lighting and architectural features like brick knee walls. A sample design is under review by the Meridian Township Sign Branding Committee.

GOAL 6: Encourage business recruitment and expansion.

- **Objective:** Coordinate with the Meridian Township EDC to supplement and review business retention methods and results. Contribute comments and observations to relevant staff and committees.

GOAL 7: Improve the Township's reputation within the business and residential community and promote communication.

- **Objective:** Develop a joint marketing strategy with involvement of the business community that brings together resources for advertising, special events and public relations.
- **Objective:** To effectively market the corridor, a branding effort must be undertaken that creates a clear, positive image to potential shoppers, visitors, investors, etc. This image must then be promoted over multiple media platforms in a comprehensive, consistent manner to educate and inform the public.
- **Objective:** Prepare marketing materials that can be presented to developers, retailers and real estate firms promoting the Corridor. This may include brochures, exhibits, trade area data, web site materials, social media activities, etc.

GOAL 8: Strive to increase business relationships with regional partners.

- **Objective:** Encourage corridor business owners to join regional partners, such as the Lansing Chamber of Commerce and the Meridian Area Business Association.
- **Objective:** Attend regional partner networking events, such as monthly MABA meetings.

SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES

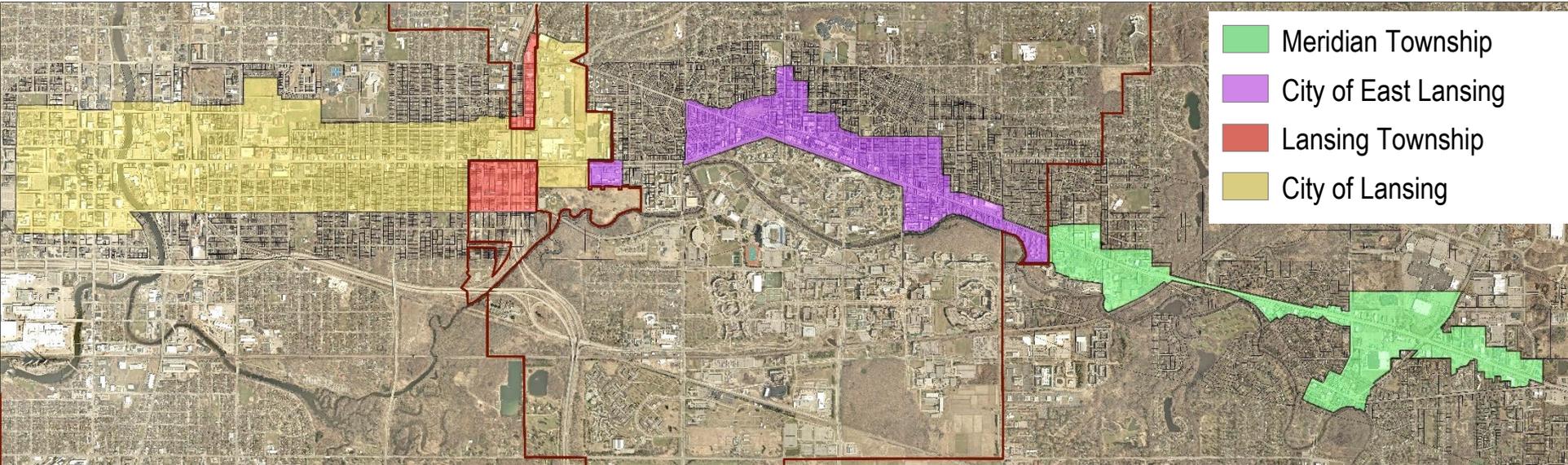
Meridian Township
Planning Commission Work Session
April 30, 2018

www.shapingtheavenue.com

Study Area

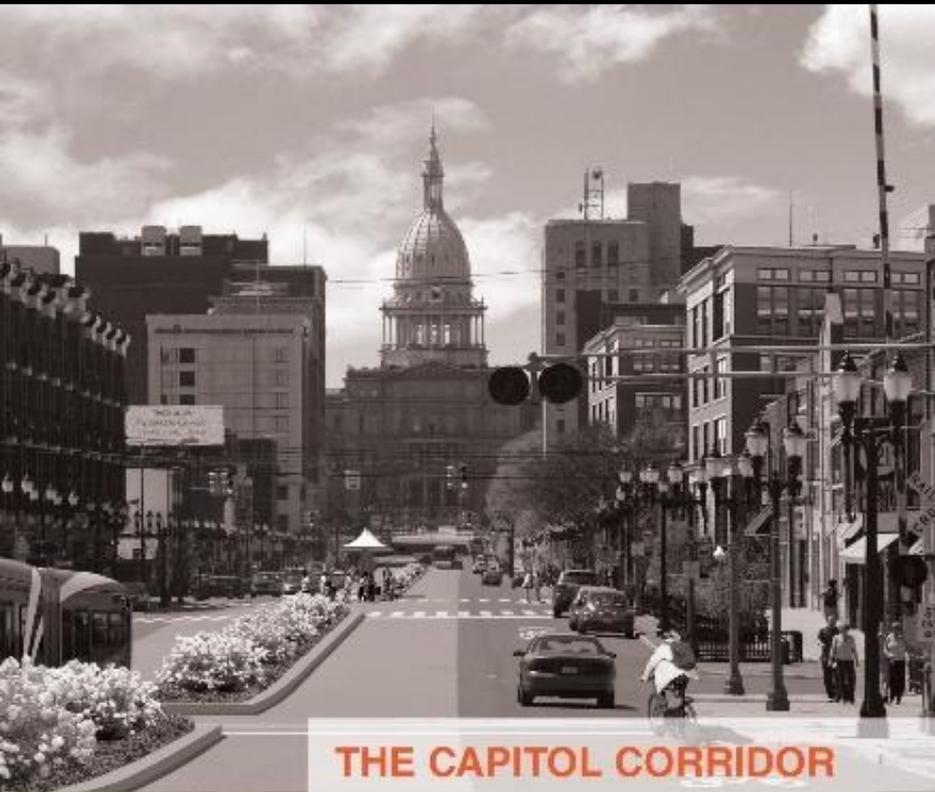
SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES



what is Shaping the Avenue?

... putting the guidelines in place that will lead the Michigan/Grand River Avenue corridor toward its full potential to support residents, businesses, cars, bikes, pedestrians and transit users.



THE CAPITOL CORRIDOR

a regional vision for
Michigan Avenue / Grand River Avenue



Building on the
visioning completed
for the
Capitol Corridor

what are form-based codes?

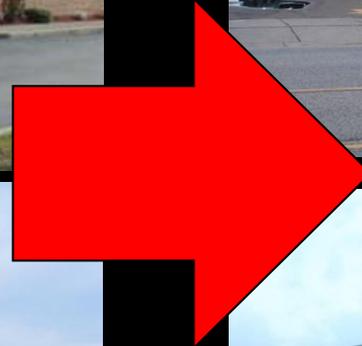
A method of regulating development to achieve a specific urban form. Form-Based Codes create a predictable public realm by controlling physical form primarily, and land uses secondarily, through city or township regulations.

Results from different zoning approaches

Conventional



Form-Based



Shaping commercial corridors



Front of bldg. faces the street/sidewalk
Parking lot is to the rear

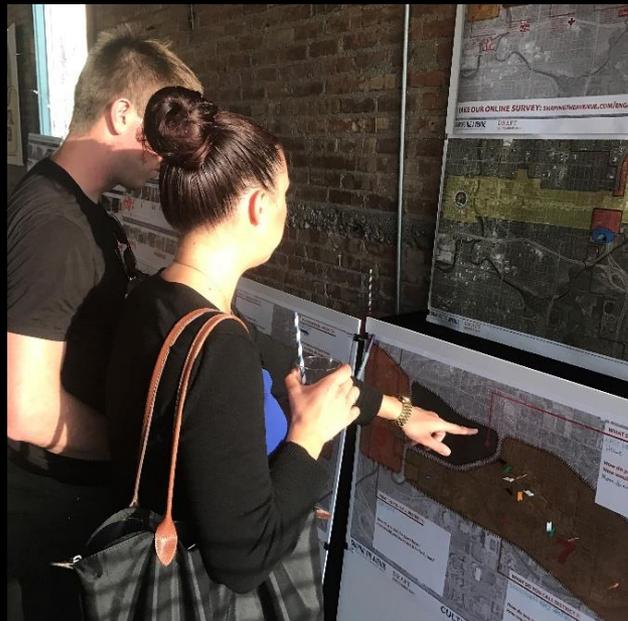


Rear of bldg. faces the street/sidewalk
Parking lot is to the front

Open House (Sept 2017)

SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES



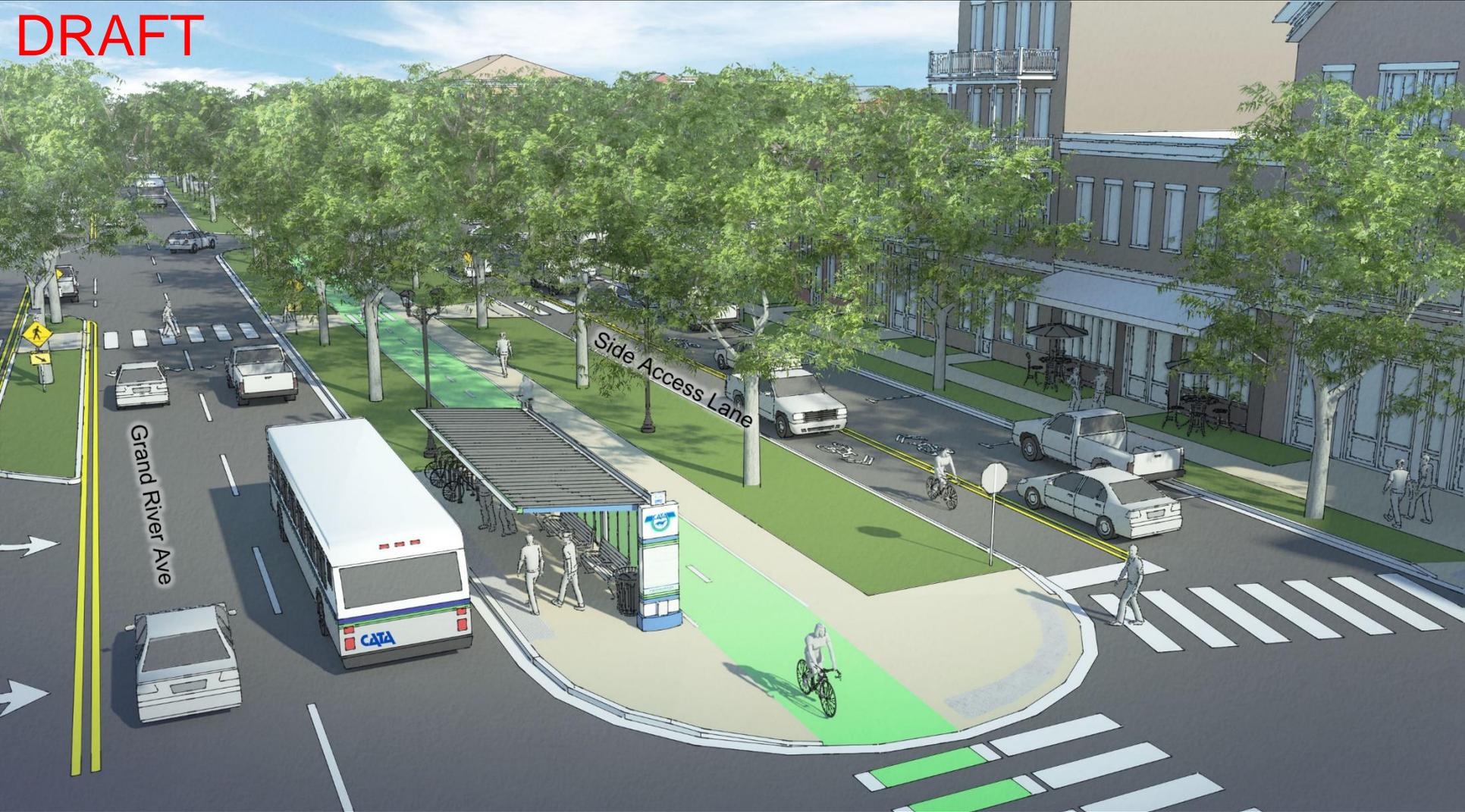
Sept 12, 2017: Young Pros Event

Sept 13, 2017: City of Lansing

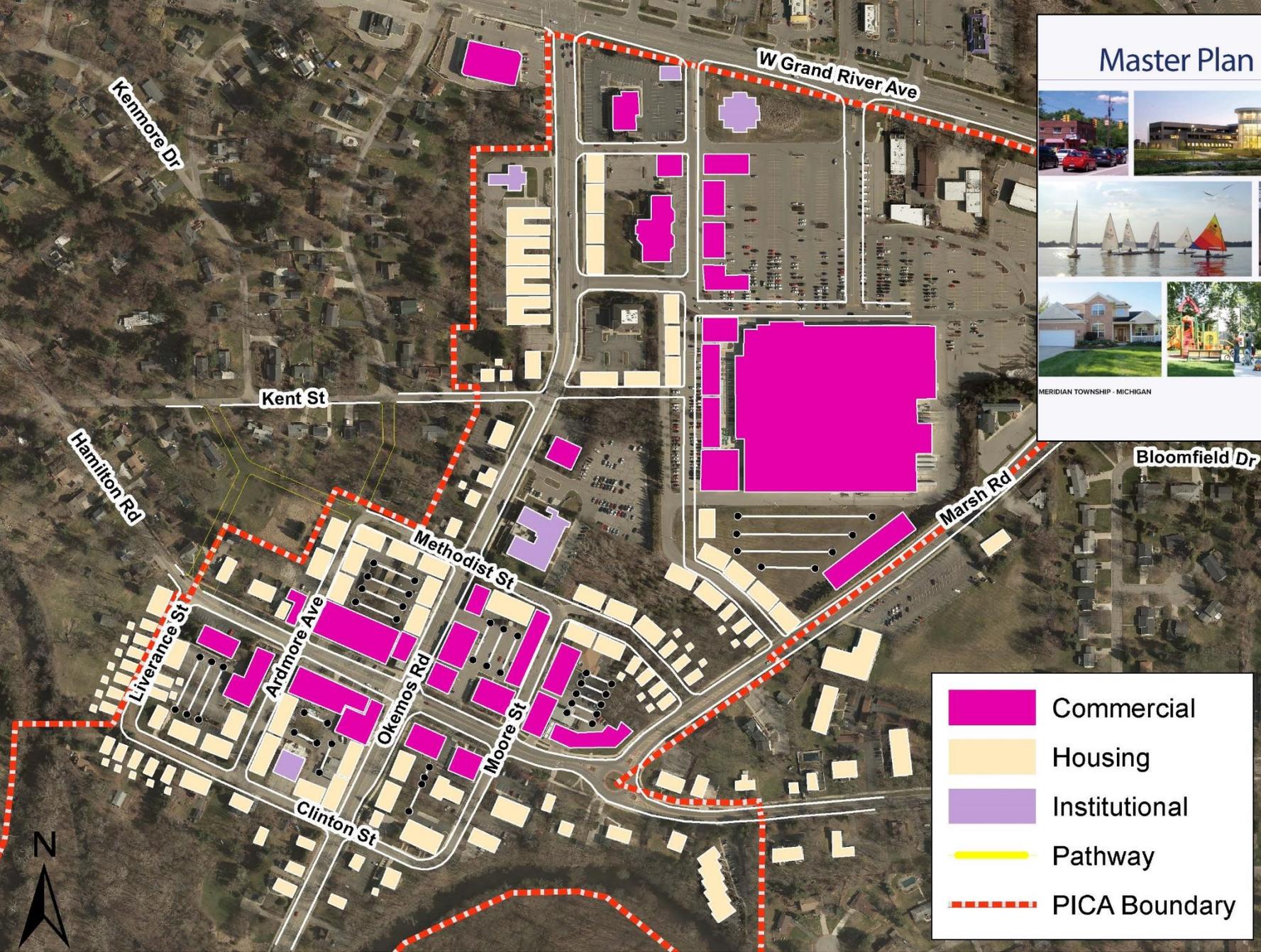
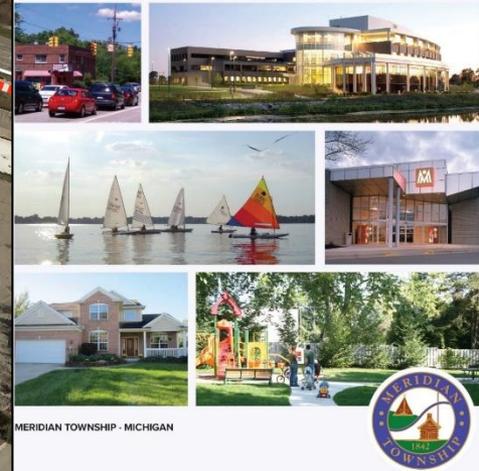
Sept 14, 2017: Meridian Township

Sept 18, 2017: East Lansing

DRAFT



Shaping the Avenue in Meridian Township



Okemos PICA. Located along Okemos Road a short distance south of Grand River Avenue, this once-thriving historic activity center is utilized as a vehicular cut-through.

Okemos Today. The street grid is desirable in scale and has the potential to support mixed use infrastructure of a walkable urban form. In its current form, Okemos lacks true connection to the active commercial corridor just minutes to the north. The center is missing an identity and purpose, even though visibility and accessibility are high.

A Bustling Downtown Tomorrow. The Okemos core will embody traditional urban living, with emphasis on creating spaces for living, working, and recreation within the neighborhood.

New development in Okemos will take advantage of its location to provide residents great access to regional transportation systems and draw in visitors with unique retail and service amenities. First floor storefronts and upper-level apartments and condos will provide an eclectic mix of users that will take advantage of plazas and street cafes to create an active streetscape.



Potential Transformation of Okemos Village

3

GOALS AND OBJECTIVES

Enhance the Viability of Township Businesses



A Upgrade commercial areas.

STRATEGIES:

1. Participate in regional efforts to redevelop the Grand River corridor, such as the Grand River Corridor Improvement Authority, and incorporate results where appropriate.
2. Improve and enhance the Haslett Road commercial area while maintaining its traditional character.
3. Encourage redevelopment of the downtown Okemos area using the Downtown Development Authority (DDA) Integrated Plan as a guide.
4. Evaluate opportunities for walkable urban design in commercial areas, including wayfinding solutions that improve usability.
5. Encourage the use of the commercial planned unit development ordinance.
6. Identify specific areas where the form-based code technique may be appropriately applied.



B Develop the Township's mixed use core into a viable and vibrant district.

STRATEGIES:

1. Explore the viability of upper floor(s) residences in the mixed use core.
2. Continue to engage the public in deciding the direction for development in the mixed use core.
3. Explore options for public-private partnerships to revitalize the Township's mixed use core.

C Improve the attractiveness of the Township's entrances and transportation corridors.

STRATEGIES:

1. Investigate grant-based funding opportunities to improve landscaping along major thoroughfares.
2. Create well-designed entrances along the main roads entering the community.
3. Promote opportunities for public art.



D Promote economic development.

STRATEGIES:

1. Communicate with and utilize the resources of the Economic Development

6

GOALS AND OBJECTIVES

Provide and Support an Efficient, Safe, and Environmentally Sensitive Multi-modal Transportation Network

A Evaluate the existing transportation network and identify problems, including traffic congestion, safety, environment, and aesthetics, which can be solved at acceptable cost.

STRATEGIES:

1. Cooperate with county, regional, and state entities to develop strategies to improve traffic flow.
2. Consider zoning and other design strategies such as Complete Streets as effective means of ensuring safe and efficient travel for all modes of transportation.

B Maintain local roads.

STRATEGIES:

1. Support improvements to local streets when a special assessment district request has been received from local residents.
2. Review reconstruction and expansion of local non-subdivision streets through the Capital Improvements Program process, when 50 percent of the improvement's funding will be paid for by the Township or its residents through special assessment districts.

C Support the Township's Complete Streets Ordinance.

STRATEGIES:

1. Share the Township's vision and work with transportation and road authorities.
2. Review current policies to identify inconsistencies and impediments to implementing the Complete Streets ordinance.
3. Conduct planning, regulatory, and physical audits of current street designs as needed and financially feasible.
4. Coordinate the Pedestrian/Bicycle Pathway Plan with Safe Routes to School policies and other Township plans.

D Expand and improve the Pedestrian/Bicycle Pathway Plan.

STRATEGIES:

1. Continue to implement the requirement for sidewalks as determined necessary.
2. Continue to implement the pedestrian/bicycle pathway system through site plan review.
3. Continue to support the pedestrian/bicycle pathway millage for construction and maintenance of the Township's pedestrian/bicycle pathway system.
4. Investigate alternative surfacing materials to reduce the amount of impervious surface materials used in the construction of the pedestrian/bicycle system.
5. Continue to investigate strategies for safe and efficient travel for bicyclists and pedestrians.
6. Coordinate the Pedestrian/Bicycle Pathway Plan with parks, schools, preserves, Greenspace Plan, neighborhoods, and other destinations.

E Encourage the use of public transportation.

STRATEGIES:

1. Encourage and promote public transportation service to any new developments and in conjunction with redevelopment via site plan review.
2. Institute design standards for site plan review to ensure public transportation access to all segments of the population but particularly those with physical disabilities.

F Coordinate Meridian Township's transportation planning with regional planning efforts administered by the Tri-County Regional Planning Commission and recorded in the 2035 Regional Transportation Plan.

STRATEGIES:

1. Coordinate transportation planning and road improvements for all non-local streets with the region's long-range plan.
2. Advocate and promote the Township's needs and interests in transportation improvements with transportation and road authorities.

7

GOALS AND OBJECTIVES

Promote efficient and sustainable growth practices

A Implement sustainable energy and environmental practices throughout the Township by utilizing the most current best practices.

STRATEGIES:

1. Budget funds to have Township buildings assessed for energy efficiency a minimum of once every five years.
2. For Meridian Township procurement purposes, when equivalent products or services are available, preference should be given to the more sustainable services or products.
3. Encourage electric car charging stations.
4. Establish a minimum level of LEED certification or the equivalent for all projects undertaken by the Township.
5. Establish ordinances providing incentives for development that emphasize redevelopment, infill development, alternative energy, and building material reuse.
6. Increase the use of alternative transportation modes for commuting and recreation by encouraging the use of mixed use planned unit developments and implementing the Greenspace Plan.
7. Explore methods, through ordinances or by providing incentives for existing businesses, to upgrade their parking lots and landscaping to increase tree cover and shade, to be energy efficient, and environmentally friendly.
8. Explore incentives for new or redeveloped projects to be LEED certified or equivalent.

DRAFT



Existing Conditions along Grand River Avenue, Meridian Township

DRAFT



Potential for change-over-time along Grand River Avenue, Meridian Township

DRAFT



Potential for change-over-time along Grand River Avenue, Meridian Township

DRAFT



Potential for change-over-time along Grand River Avenue, Meridian Township



DRAFT



Potential for change-over-time along Grand River Avenue, Meridian Township

Draft Code Elements:

1. Purpose and Intent
2. Administration and Applicability
3. Regulating Plans
4. Building Form Standards
5. Public Realm Standards
6. Architectural Standards
7. Parking Standards
8. Building Uses
9. Definitions

Part 1: Purpose & Intent

- Create more walkable pedestrian-friendly and transit-supportive mixed-use environments in the Okemos Village center and along the Grand River Avenue corridor
- Places a primary emphasis on placemaking (physical form and character), and has a secondary focus on land uses

Part 2: Administration

- Grand River Avenue Form District
 - **OPTIONAL**: if opt in, must follow standards
- Okemos Village Form District
 - **MANDATORY**: thresholds for compliance (p 5)
- Establishes process and authority for review / approval

your questions:

What are the benefits to opting in to the form-based code?

- Expedited approvals: fewer special uses and public hearings for projects that comply
- Increased height/density, reduced parking
- Allow greater mixture of uses
- Recommended: Fewer studies required (e.g. traffic impacts) for mixed use
- Recommended: Amend C District, restrict uses / add MU-PUD design standards

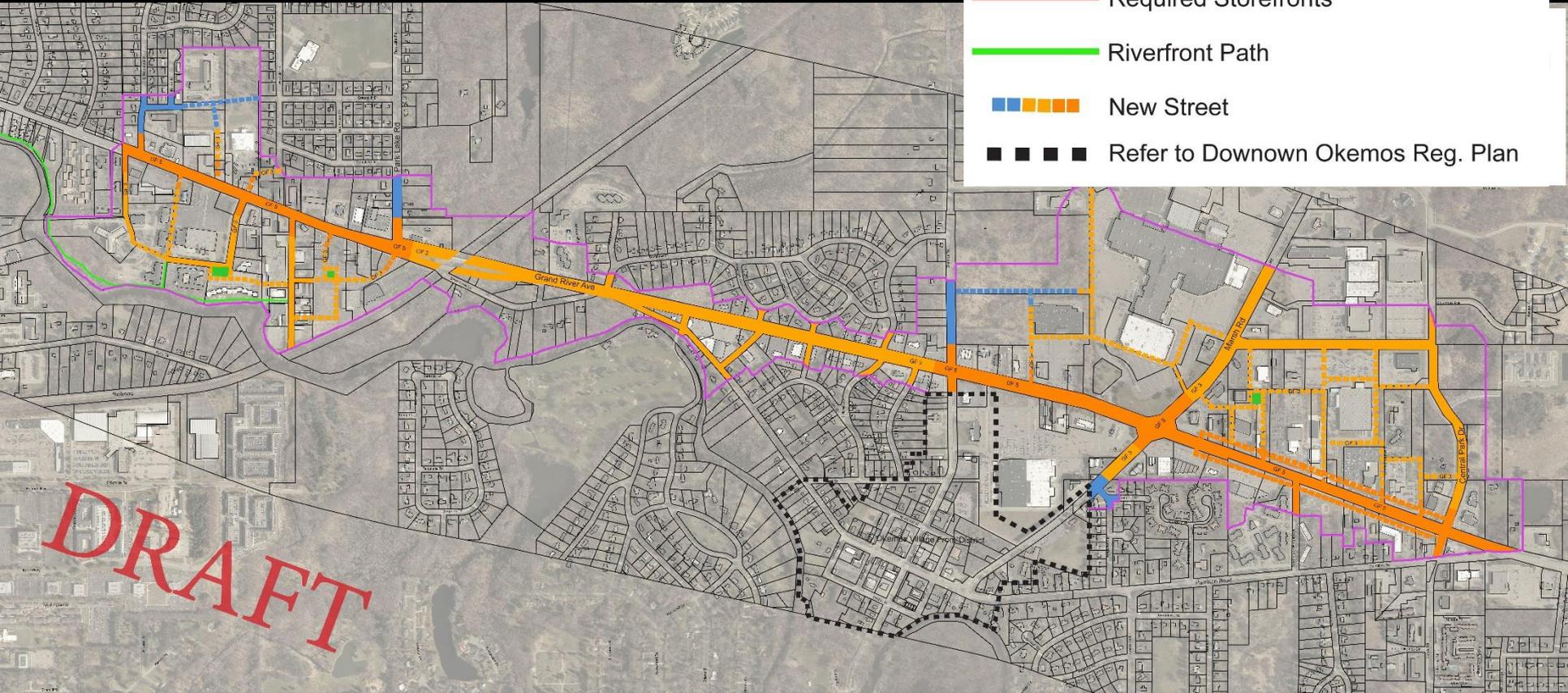
Draft Code Elements:

1. Purpose and Intent
2. Administration and Applicability
3. Regulating Plans
4. Building Form Standards
5. Public Realm Standards
6. Architectural Standards
7. Parking Standards
8. Building Uses
9. Definitions

Parts 3 to 6
define form &
character

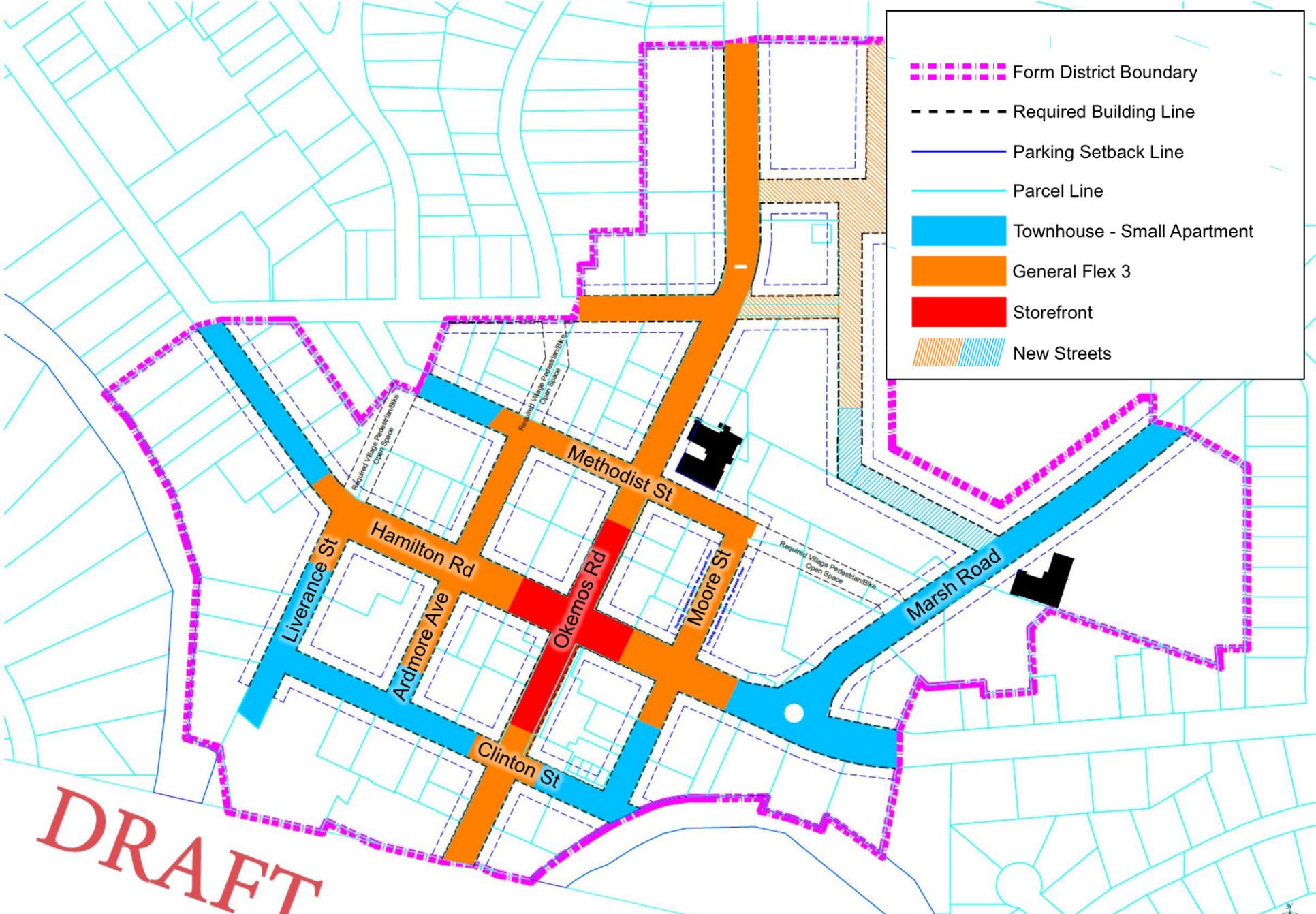
Part 3: Regulating Plans

- Required Open Space
- Townhouse - Small Apt.
- General Flex 3 (Number = Max Height)
- General Flex 5
- District Boundary
- Required Storefronts
- Riverfront Path
- New Street
- Refer to Downtown Okemos Reg. Plan



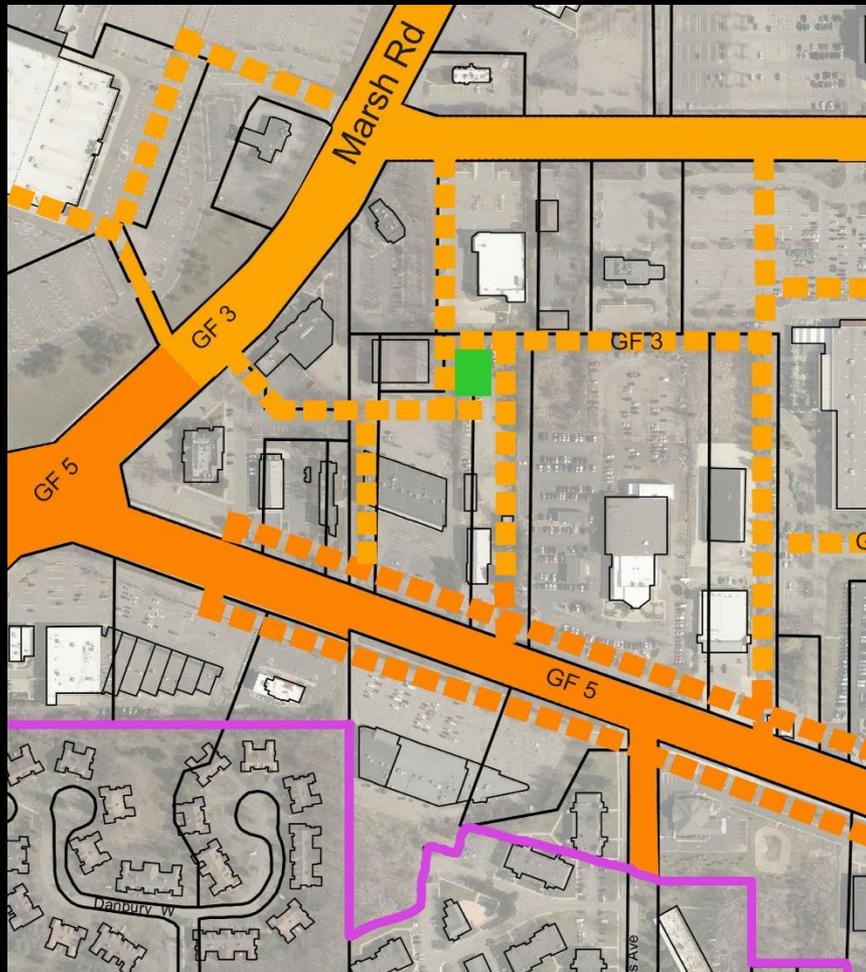
DRAFT

Grand River Avenue Form District Regulating Plan



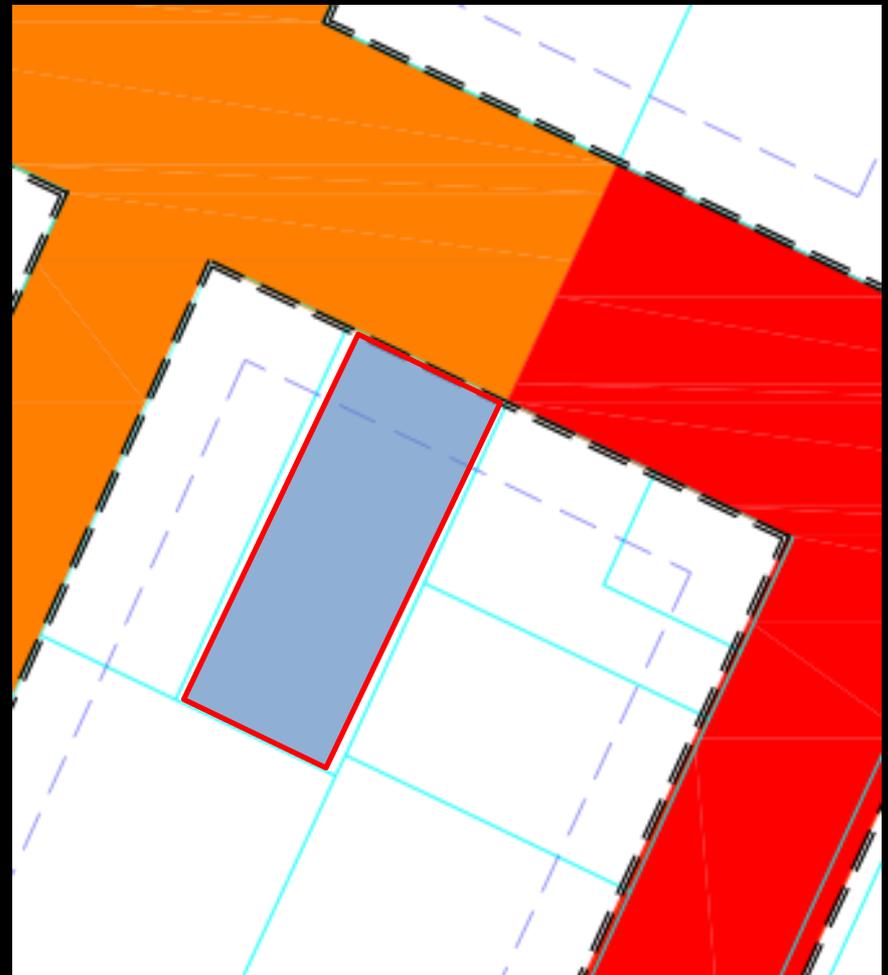
-  Form District Boundary
-  Required Building Line
-  Parking Setback Line
-  Parcel Line
-  Townhouse - Small Apartment
-  General Flex 3
-  Storefront
-  New Streets

DRAFT



Grand River Reg Plan:

- Sets district boundary
- Assigns Building Form (Part 4)
- Locates future streets (Part 5)



Okemos Reg Plan:

- Same as Grand River *plus*
- Locates RBL (Required Building Line)
- Locates Parking Setback Line



Parking Setback

RBL

3815 gross sq ft
55' FOOTPRINT

Part 4: Building Form Standards

	Required Open Space
	Townhouse - Small Apt.
	General Flex 3 (Number = Max Height)
	General Flex 5
	District Boundary
	Required Storefronts
	Riverfront Path
	New Street
	Refer to Downtown Okemos Reg. Plan

- Frontage Type (from Reg Plan): rules about **height, placement, building elements, uses**
 - Townhouse / Small Apartment
 - General Flex Avenue
 - General Flex Village
 - Storefront (Village only)

403. General Flex Village

ILLUSTRATIONS AND INTENT

Note: These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards on the following pages for the specific prescriptions and restrictions of the General Flex Building form standard.

This is the basic urban STREET-SPACE FRONTAGE. It fully defines the street edge and accommodates a range of uses, including retail, office, institutional, and residential. This frontage is in the most intense areas, generally close to the center of the District. It is anticipated that there will be significant pedestrian traffic along this frontage type.



Mixed-use building entrance



Residential building



Mixed-use buildings



Storefront streetscape



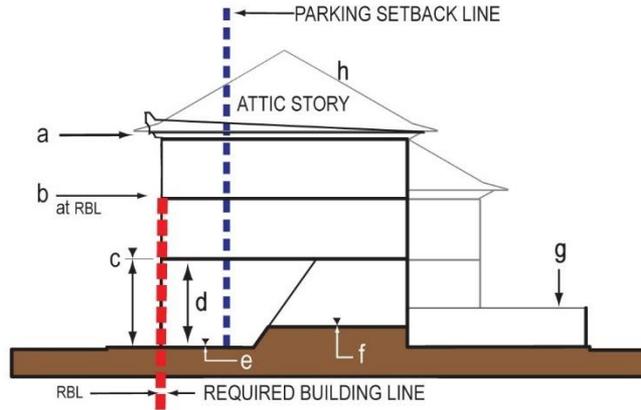
Residential building entrance



Storefront entrance



Mixed-use buildings with 'tower'

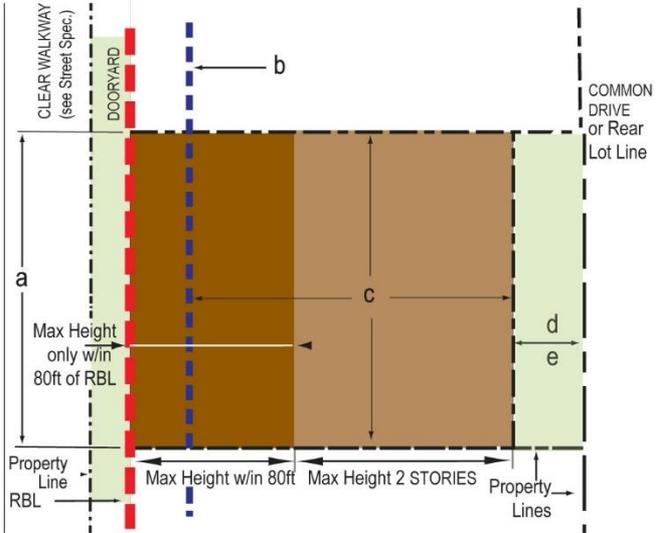


HEIGHT

		General Flex 3
a	Max STORY Height ¹	3
b	Min STORY Height	2
	Ultimate Building Height ¹	35'
c	2nd Floor Elevation ²	18' to 22'
d	Ground Floor Clear Height, COMMERCE ³	-4" to +18"
e	Ground Floor Elevation, COMMERCE ³	15' min
f	Ground Floor Elevation, Residential Units ⁴	3' to 8'
g	PRIVACY FENCE	7' Max
	STREET WALL	4' to 8'
h	Visible Roof Pitch ⁵	Min 4/12 Max 12/12, MANSARD configurations: lower pitch: Min 20/12 Max 45/12 upper pitch: Min 4/12 Max 8/12

Notes:

1. Where a BONUS STORY is awarded by the Township, the STORY height will increase by 1 and the Ultimate Building Height by 10'.
2. Measured from average front sidewalk elevation to the 2nd story finished floor. This is to ensure adequate height



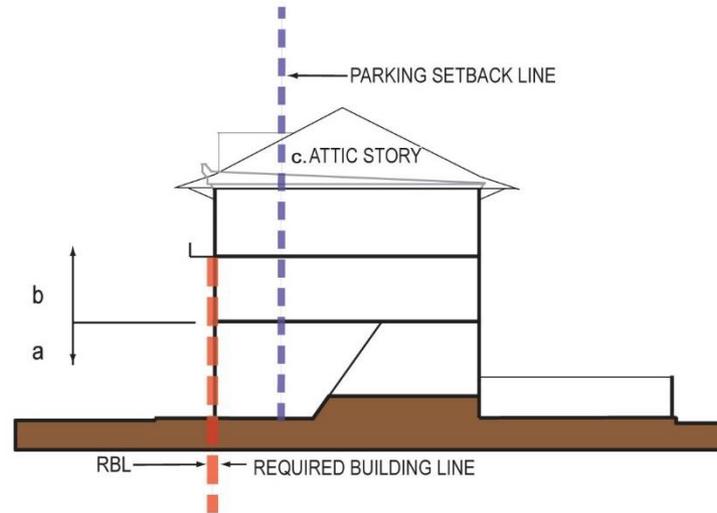
PLACEMENT

		General Flex 3
a	Frontage Build-To, Min	70%
b	PARKING SETBACK LINE ¹	30'
c	PRIVATE OPEN AREA, Location, Min Area ²	10%
	Side Setbacks	None
d	Rear Setback, COMMON DRIVE	8'
e	Rear Setback, no COMMON DRIVE	25'
	STREET WALL	Required on unbuilt RBL
	PRIVACY FENCE ³	Permitted, Side or Rear Lot Lines
	Footprint Max. ⁴	15,000 sf

Notes:

1. Reduced to 12ft where a minimum 5ft STREET WALL is built.
2. At least 67% of the PRIVATE OPEN AREA is required to be at grade.
3. Except where NEIGHBORHOOD MANNERS is in effect, see *Section 405.B*.
4. For grocery stores, the maximum footprint is doubled.

General Flex Village

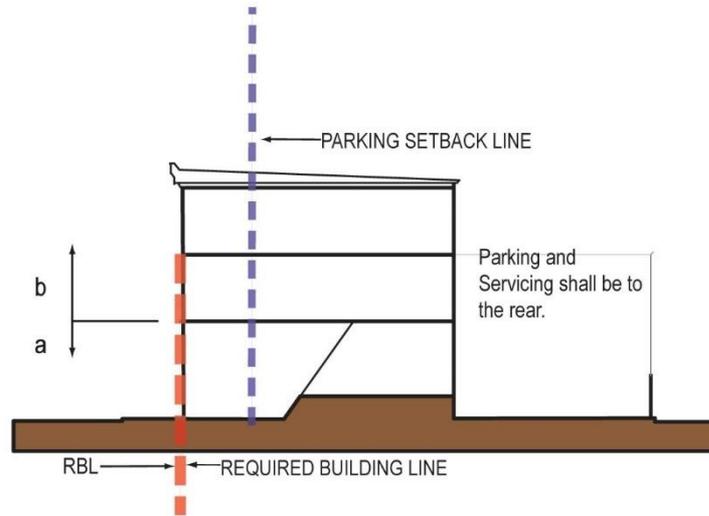


ELEMENTS

		General Flex 3
a	Ground STORY FENESTRATION	40 to 80%
b	Upper STORY FENESTRATION	25 to 80%
c	ATTIC STORY	One level permitted per 402.C.3
	FAÇADE: Max door to door ¹	75'

Notes:

1. On the FAÇADE of an individual building or FAÇADE COMPOSITION.



USES

		General Flex 3
a	Ground STORY ¹	COMMERCE, RESIDENTIAL ²
b	Upper STORIES ³	COMMERCE, RESIDENTIAL

Notes:

1. Support functions for RESIDENTIAL and hotel uses, such as lobbies, rental offices, and club rooms may be located at grade.
2. Street entrances shall be at grade, with transitions to meet the minimum finished floor elevation for RESIDENTIAL units within the building interior.
3. No COMMERCE (INCLUDING RETAIL) above RESIDENTIAL.

- General Provisions that apply to all (start p 16):
 - Height
 - Placement
 - Elements
 - On-site Parking and Access (curb cuts)
 - Frontage Designation Flexibility
 - Civic Buildings

- Special Requirements for General Flex and Storefront Frontages (start p 33):
 - Façade Composition
 - Neighborhood Manners
 - Bonus Height
 - Private Open Area

your questions:

Why is residential ground finished floor raised?



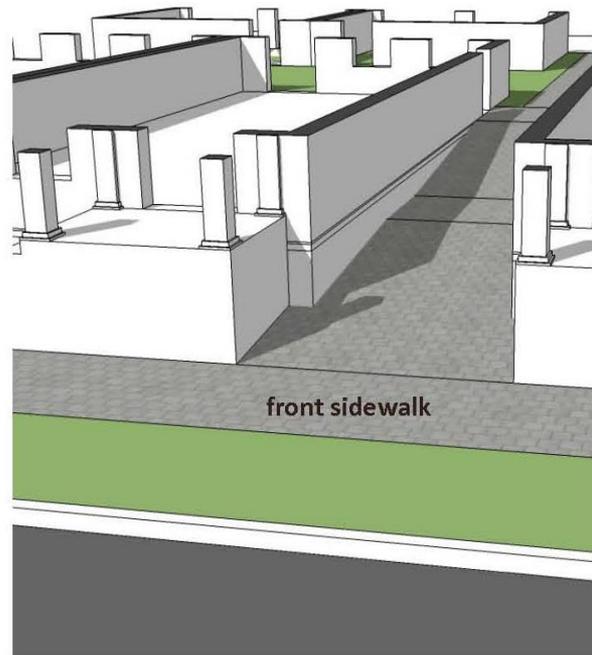
Without raised finished floor: reduced privacy; ground-floor units less desirable; reduced unit values; curtains stay closed; fewer eyes on the street

your questions:

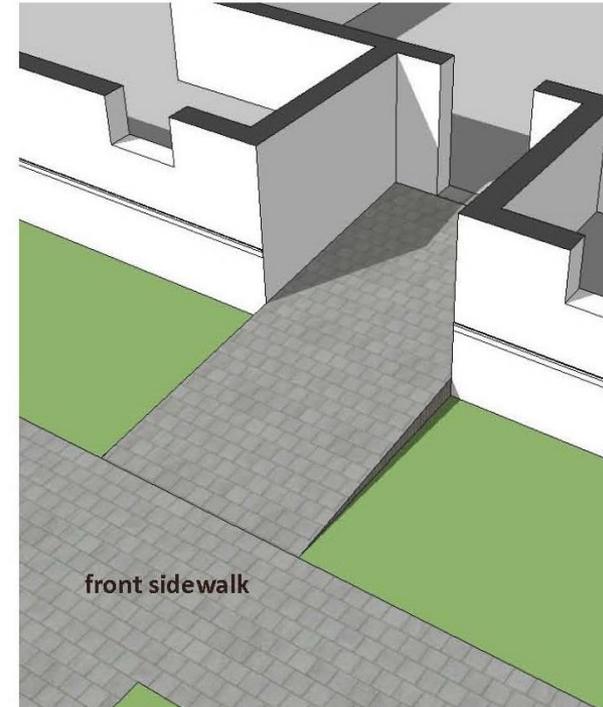
How do you make the building accessible?



Elevated first finished floor accessed by interior ramp



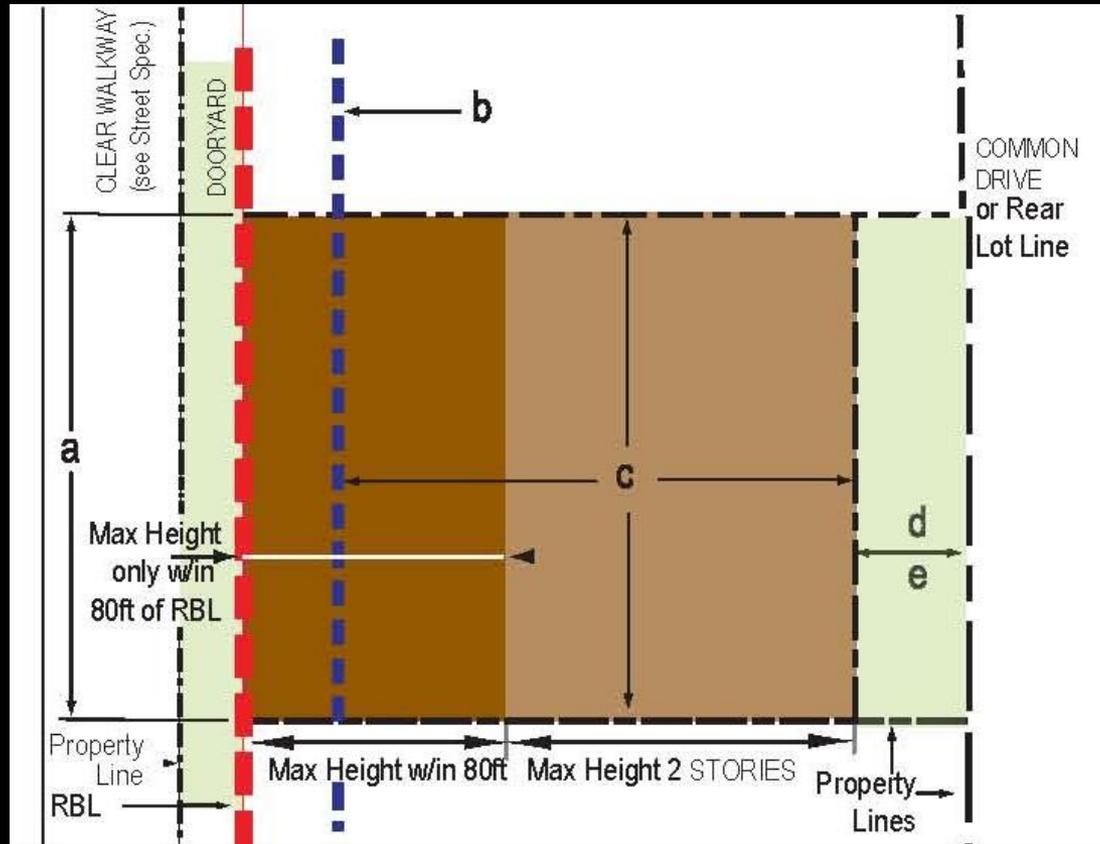
A subtle and well-integrated ramp set behind the building's front facade plane



Ramp from the front sidewalk to the front door

your questions:

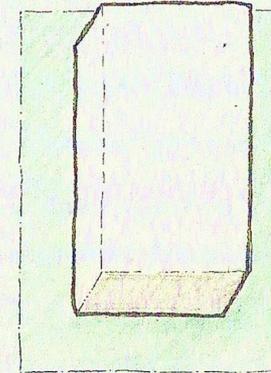
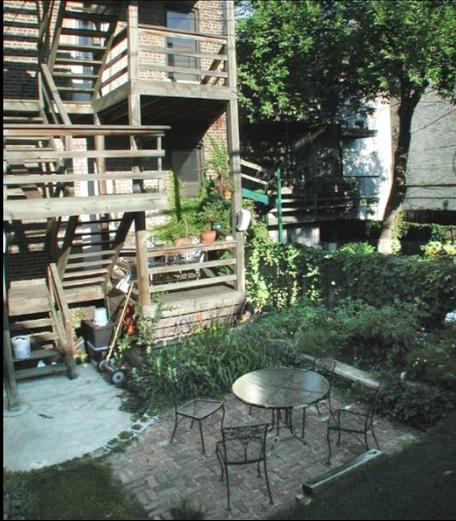
What is private open space, how does it work?



In General Flex: 10% of area "C", Townhouse: 15% of area "C"
67% in no more than 2 areas; Gen Flex 5 may be above grade

your questions:

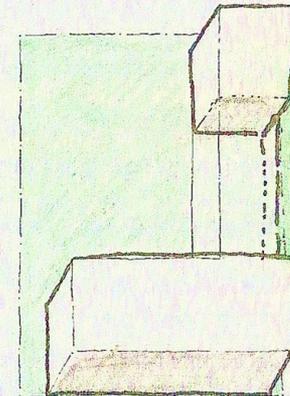
What is private open space, how does it work?



**CONVENTIONAL
DEVELOPER
SITING**

3,000 sq. ft Bldg.
7,000 sq. ft Lot

Front, Side and
Rear Setbacks.



**"URBAN"
SITING**

3,000 sq. ft Bldg.
7,000 sq. ft Lot

Front and Side
Build-To's

your questions:

Where do Neighborhood Manners apply?

General Flex

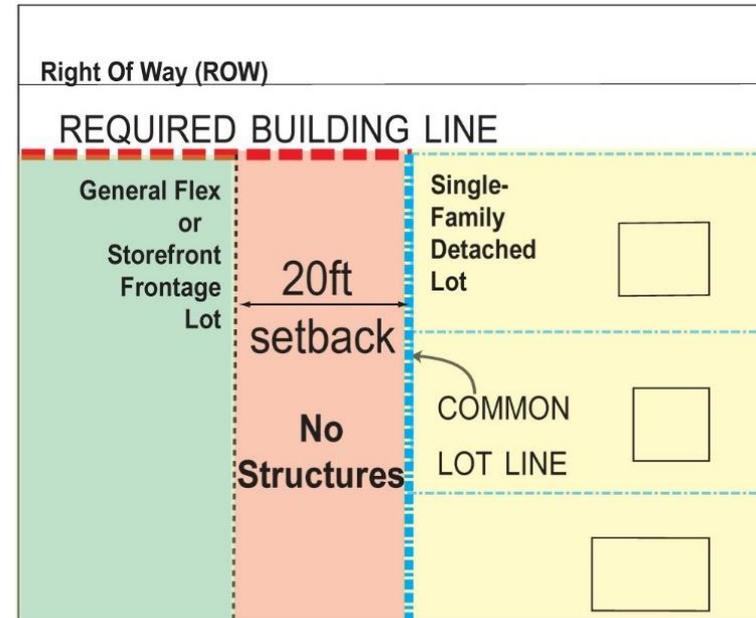
B. Neighborhood Manners

Additional rules to ensure a comfortable and complementary relationship with adjacent single family detached dwellings.

For the General Flex and Storefront BUILDING FORM STANDARD frontages sharing a COMMON LOT LINE with a single-family residential zoning district outside the Form District the following rules apply.

1. A GARDEN WALL, 4 to 8 feet in height, shall be constructed within one (1) foot of the COMMON LOT LINE.
2. Trees from the *Street Tree List* shall be planted, on maximum 30-foot centers, between 5 and 10 feet from this wall.
3. Neighborhood Manners Setbacks

Storefront



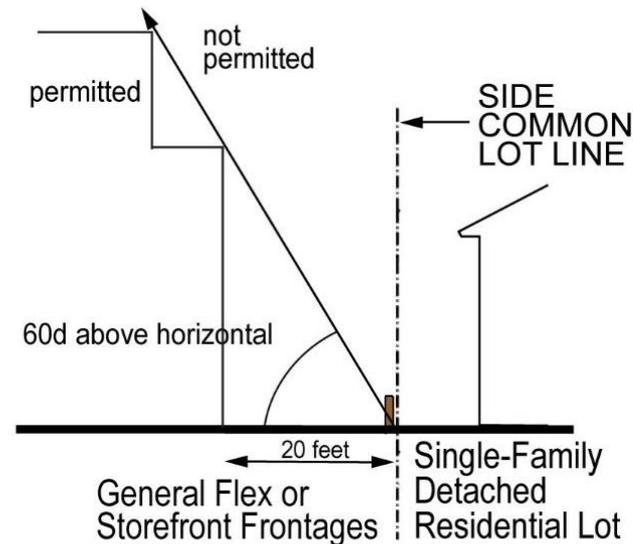
405.B.3.a Neighborhood Manners 20ft Setback

3. Neighborhood Manners Setbacks

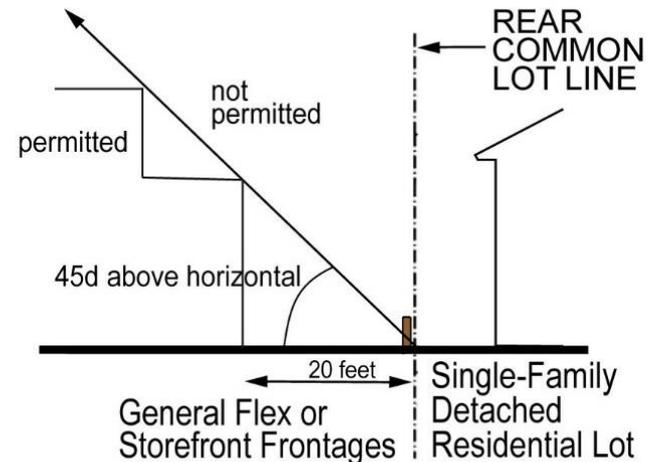
- a. There shall be a 20 foot setback from the COMMON LOT LINE. Trees and surface parking are permitted within this setback, but there shall be no structures within this area. (See *diagram 4.05.B.3.a*) If the setback area is used as a COMMON DRIVE the trees required by this section may be planted within this area.
- b. There shall be an additional setback plane:
 - (i) For COMMON LOT LINES that are within 80 feet of and perpendicular to a REQUIRED BUILDING LINE, extending at an angle of 60 degrees above horizontal, beyond which no building or structure is permitted.
 - (ii) For COMMON LOT LINES that are more than 80 feet from a REQUIRED BUILDING LINE, extending at an angle of 45 degrees above horizontal, beyond which no building or structure is permitted.

4. The Neighborhood Manners Setback Area shall be adjusted with any frontage change per *402.E Frontage Designation Flexibility*.

405.B.3.a Neighborhood Manners 20ft Setback



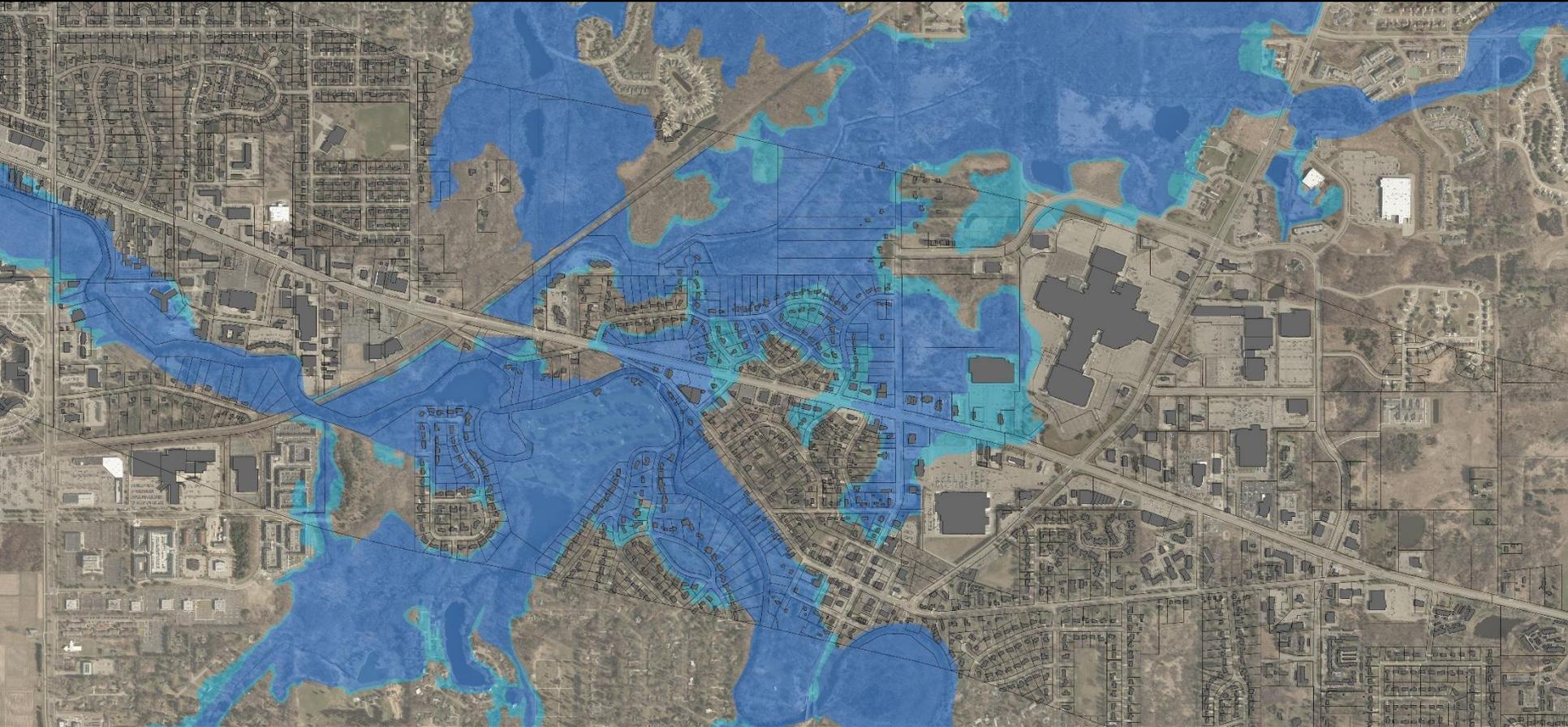
405.B.3.b.i Neighborhood Manners Setback Plane.



405.B.3.b.ii Neighborhood Manners Setback Plane.

your questions:

How is the RBL impacted by the presence of the floodplain?



Part 5: Public Realm Standards

- Street Type Specifications
- Bus Stop Connectivity & Location Guidelines
- Mid-Block Pedestrian Crossing Guidelines
- Streetscape Standards
- Squares and Civic Greens

Public Realm Standards - Applicability

- Intent: Promote a more walkable street system in collaboration with applicants, Ingham County Roads Department, and MDOT
- Township CIA or DDA Right-of-Way and public space projects
- Streetscapes for new developments
- Road reconstruction projects
- Any new street / side access lane or redesigned street



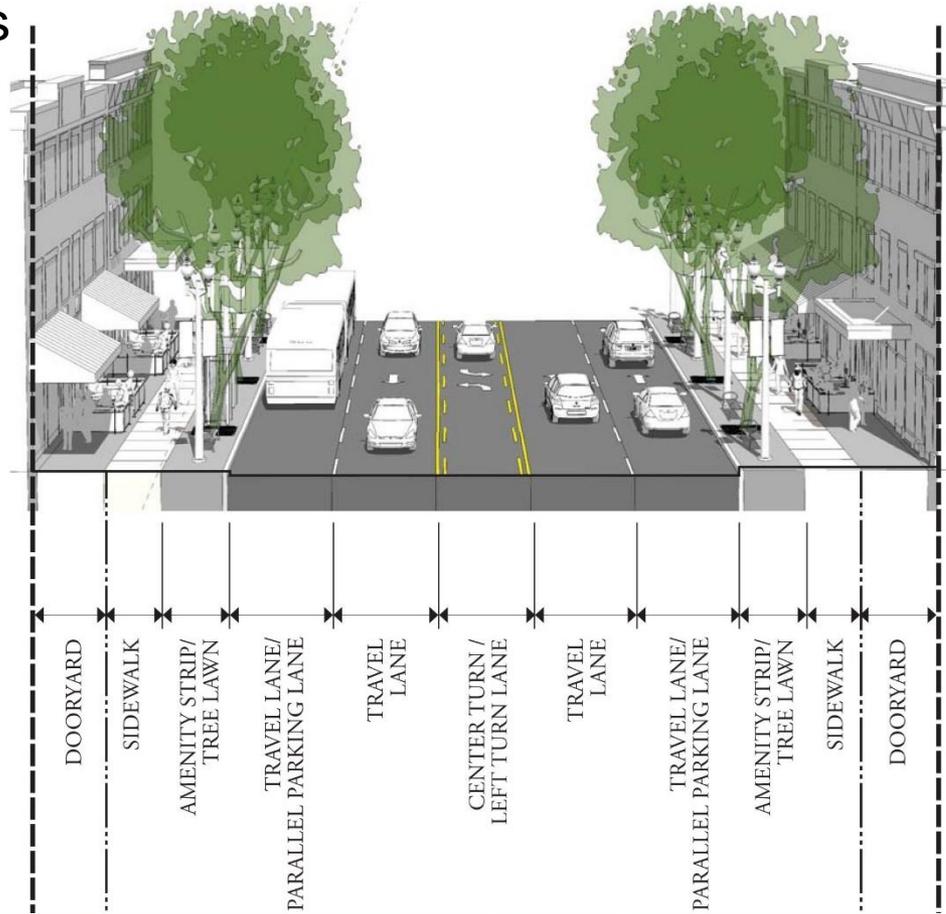
LEGEND

-  NS 80-46
-  NS 82-54
-  NS 78-44
-  NS 70-36
-  NS 70-36 (Proposed Street)
-  MA 252-54
-  MA 140-54

NOTE:

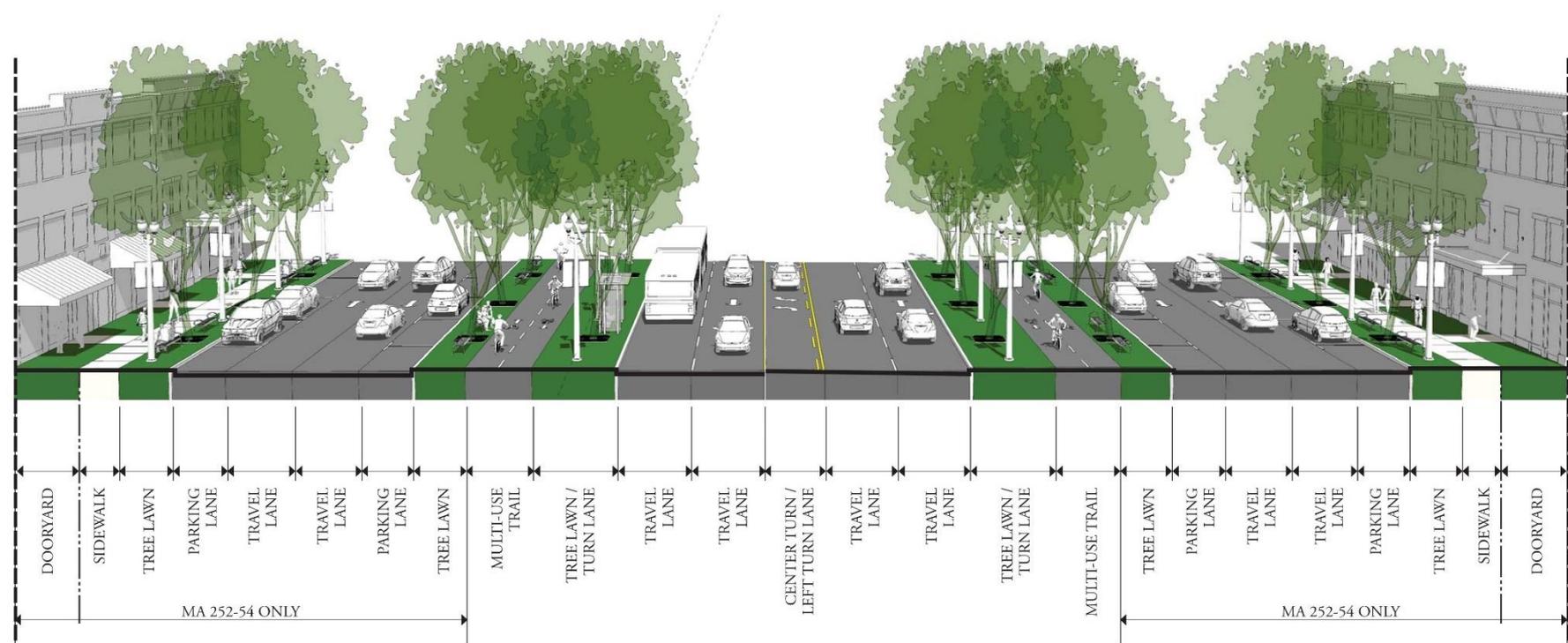
The numbers refer to dimensions within the STREET-SPACE. The first number is the literal STREET-SPACE (the distance between FAÇADES) and the second is the distance curb-face to curb-face (see cross-section specifications at the end of this section).

Neighborhood Streets



Street Type	Min. Right-of-Way	Travel Lane Width	Center Turn Lane Width	Parallel Parking Lane Widths	Amenity Strip Width	Tree Lawn Width	Sidewalk Width	Multi-Use Trail Width	Dooryard	Total Minimum Street Space
NS 80-46	78'	11'	10'	7' (optional) ¹	8' Min.	-	7'	-	1'	80'
NS 82-54	80'	11'	10'	-	6' Min.	-	6'	-	1'	82'
NS 78-44	76'	11'	-	7' (optional) ¹	6' Min.	-	6'	-	1'	78'
NS 70-36	60'	10'	-	8'	-	6' min.	6'	-	5'	70'
MA 252-54	232'	11'	10'	8'	8' Min.	-	6'	10'	10'	252'
MA 140-54	110'	11'	10'	-	12' Min.	-	-	10'	15'	140'

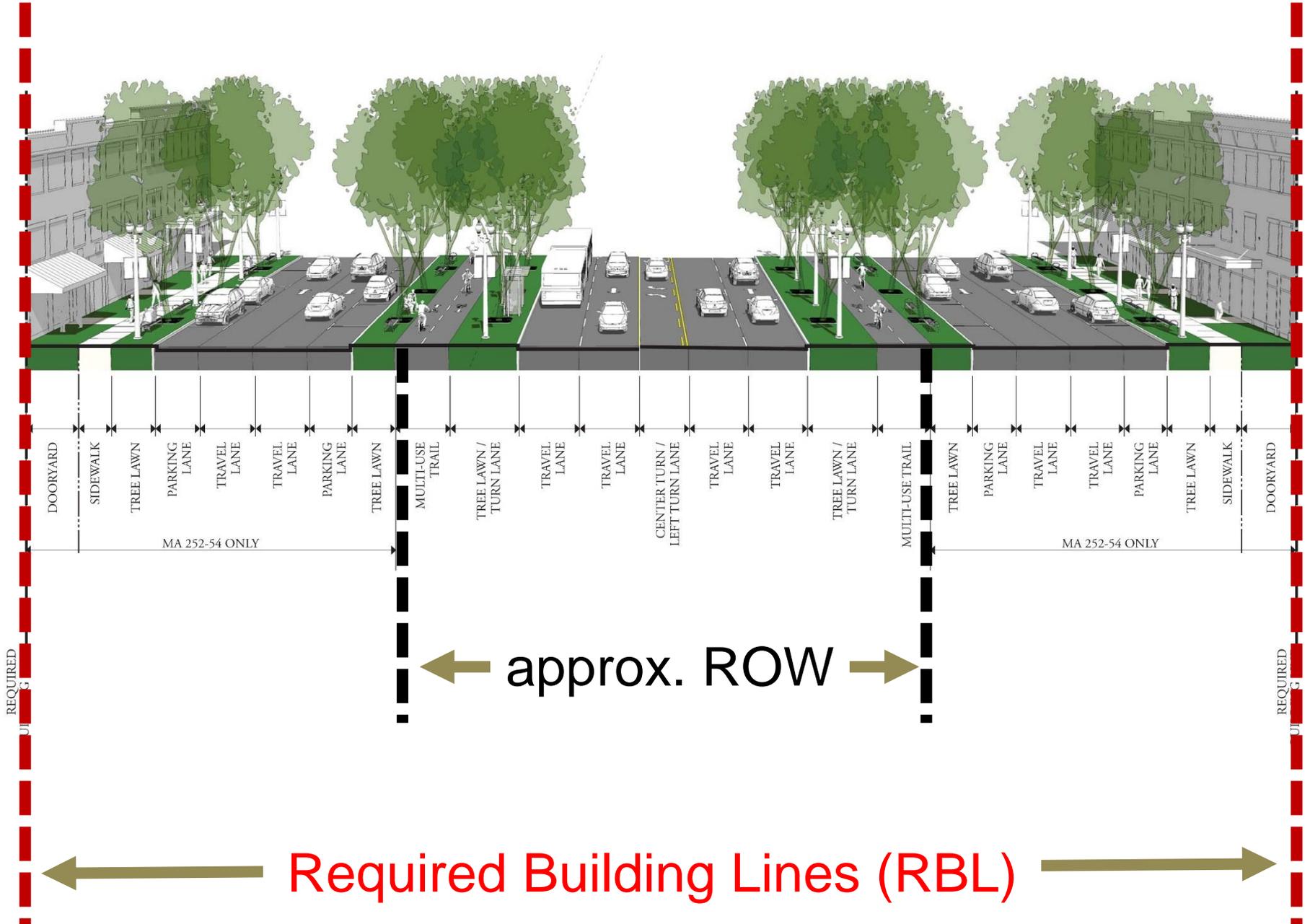
Major Arterial Street (Grand River Ave)



REQUIRED BUILDING LINE

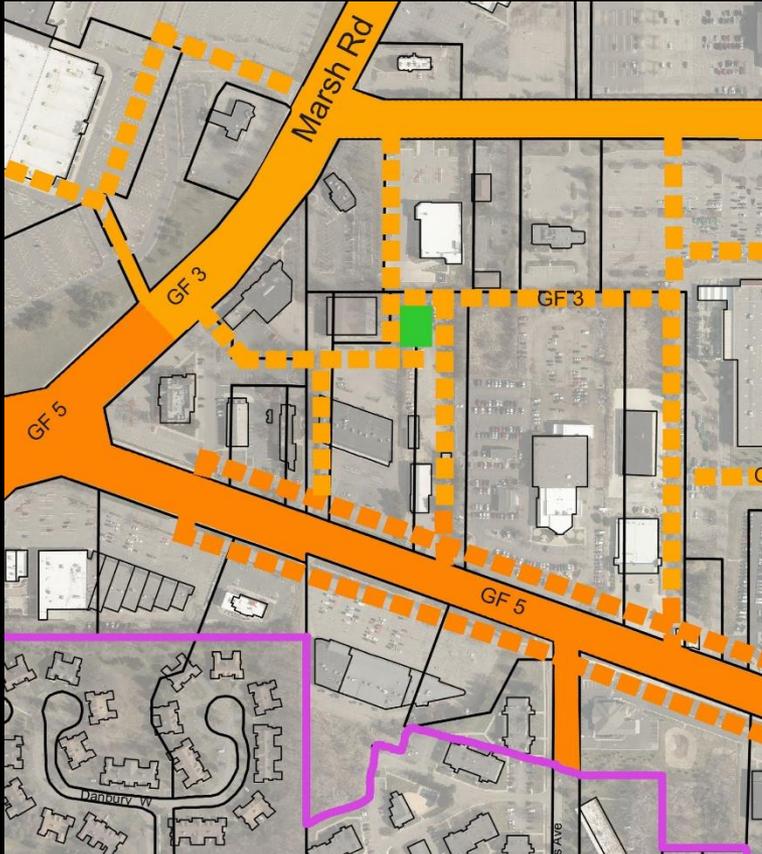
REQUIRED BUILDING LINE

Street Type	Min. Right-of-Way	Travel Lane Width	Center Turn Lane Width	Parallel Parking Lane Widths	Amenity Strip Width	Tree Lawn Width	Sidewalk Width	Multi-Use Trail Width	Dooryard	Total Minimum Street Space
NS 80-46	78'	11'	10'	7' (optional) ¹	8' Min.	-	7'	-	1'	80'
NS 82-54	80'	11'	10'	-	6' Min.	-	6'	-	1'	82'
NS 78-44	76'	11'	-	7' (optional) ¹	6' Min.	-	6'	-	1'	78'
NS 70-36	60'	10'	-	8'	-	6' min.	6'	-	5'	70'
MA 252-54	232'	11'	10'	8'	8' Min.	-	6'	10'	10'	252'
MA 140-54	110'	11'	10'	-	12' Min.	-	-	10'	15'	140'



your questions:

How do Regulating Plan and Street Atlas work?



Section 86-625
Oleaves Village and Grand River Avenue Town Districts

403. General Flex Village

ILLUSTRATIONS AND INTENT

Note: These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards on the following pages for the specific prescriptions and restrictions of the General Flex Building form standard.

This is the basic urban STREET-SPACE FRONTAGE. It fully defines the street edge and accommodates a range of uses, including retail, office, institutional, and residential. This frontage is in the most intense areas, generally close to the center of the District. It is anticipated that there will be significant pedestrian traffic along this frontage type.

- Mixed-use building entrance
- Residential building
- Mixed-use buildings
- Streetfront streetscape
- Residential building entrance
- Streetfront entrance
- Mixed-use buildings with tower

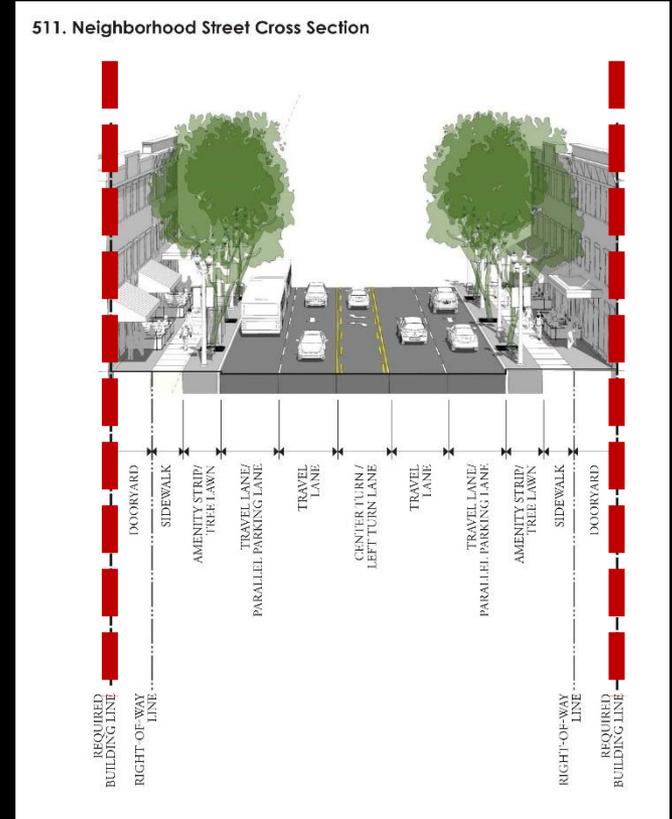
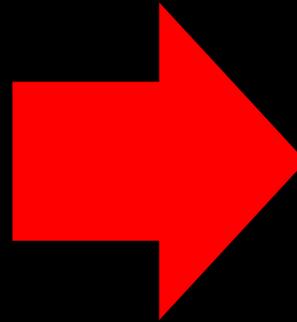
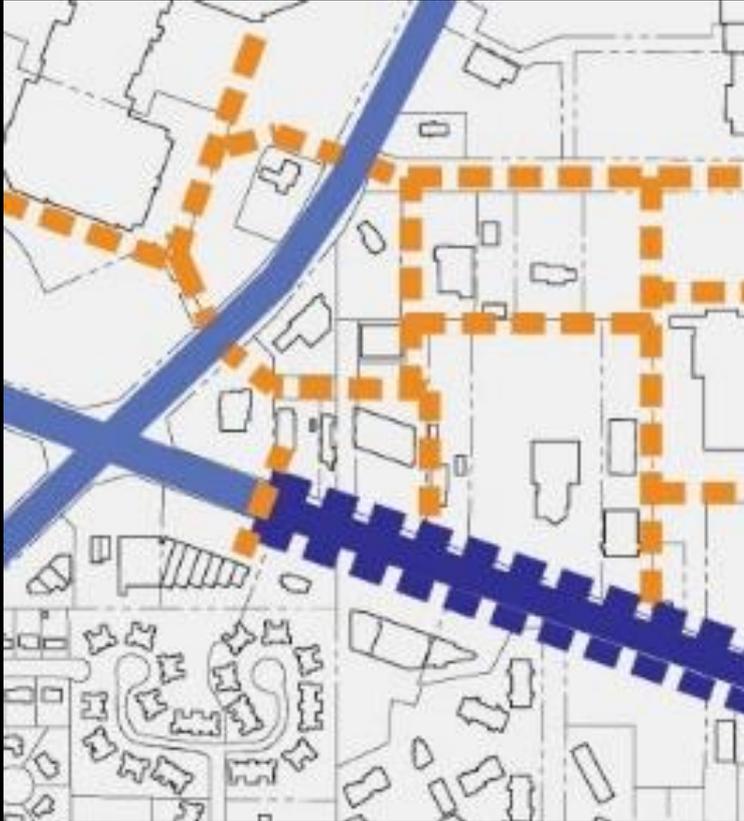
February 2018
REVIEW DRAFT 2

Shaping the Avenue
Mendota Township 27

Regulating Plan assigns **Frontage Type** and location of **new streets**

your questions:

How do Regulating Plan and Street Atlas work?



Street Atlas assigns **Street Type** and location of **RBL**

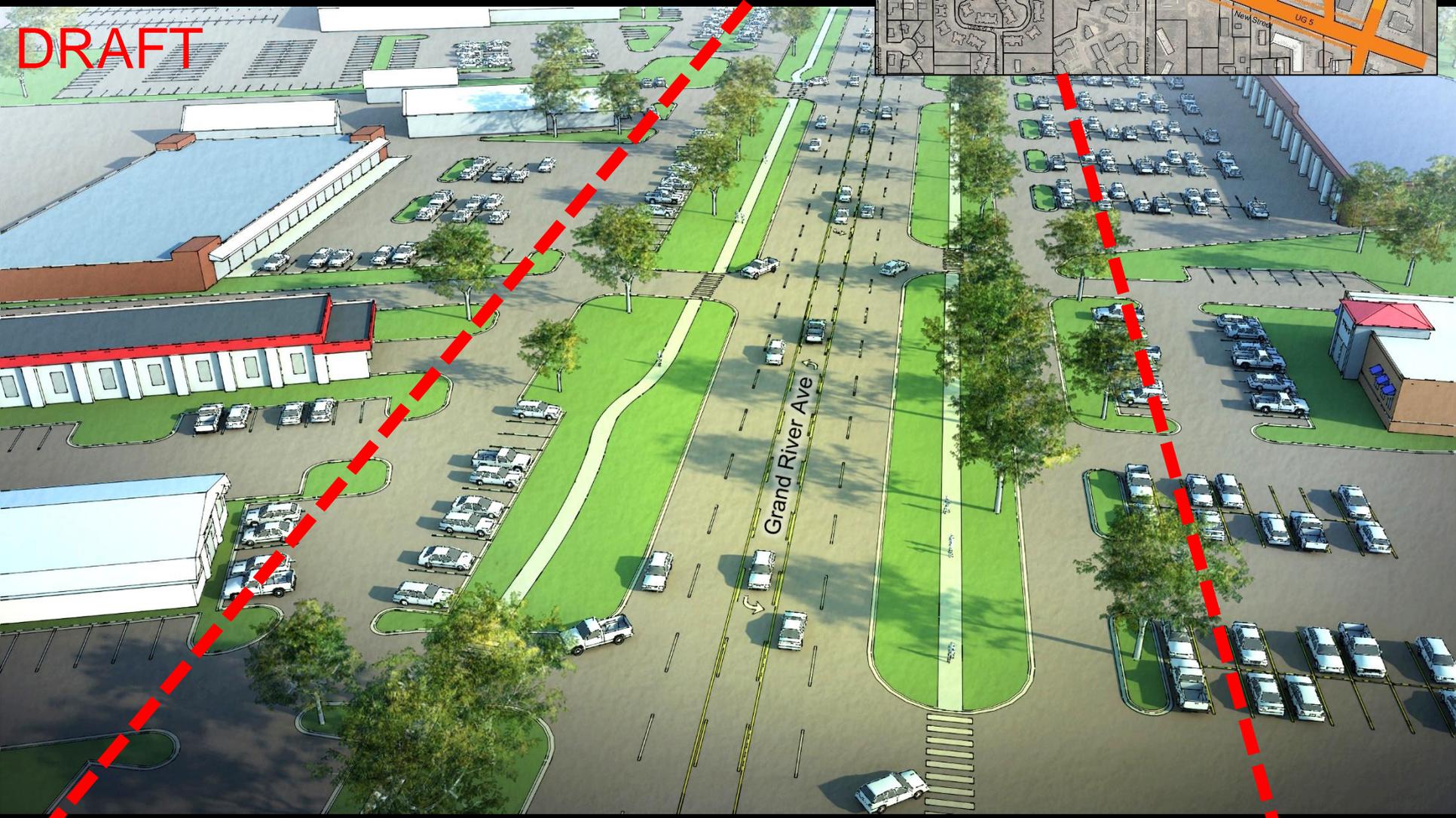
your questions:

What if Grand River Ave or other street designs are not implemented? The Township has no control over the roads.

Will the code work with the existing street designs?

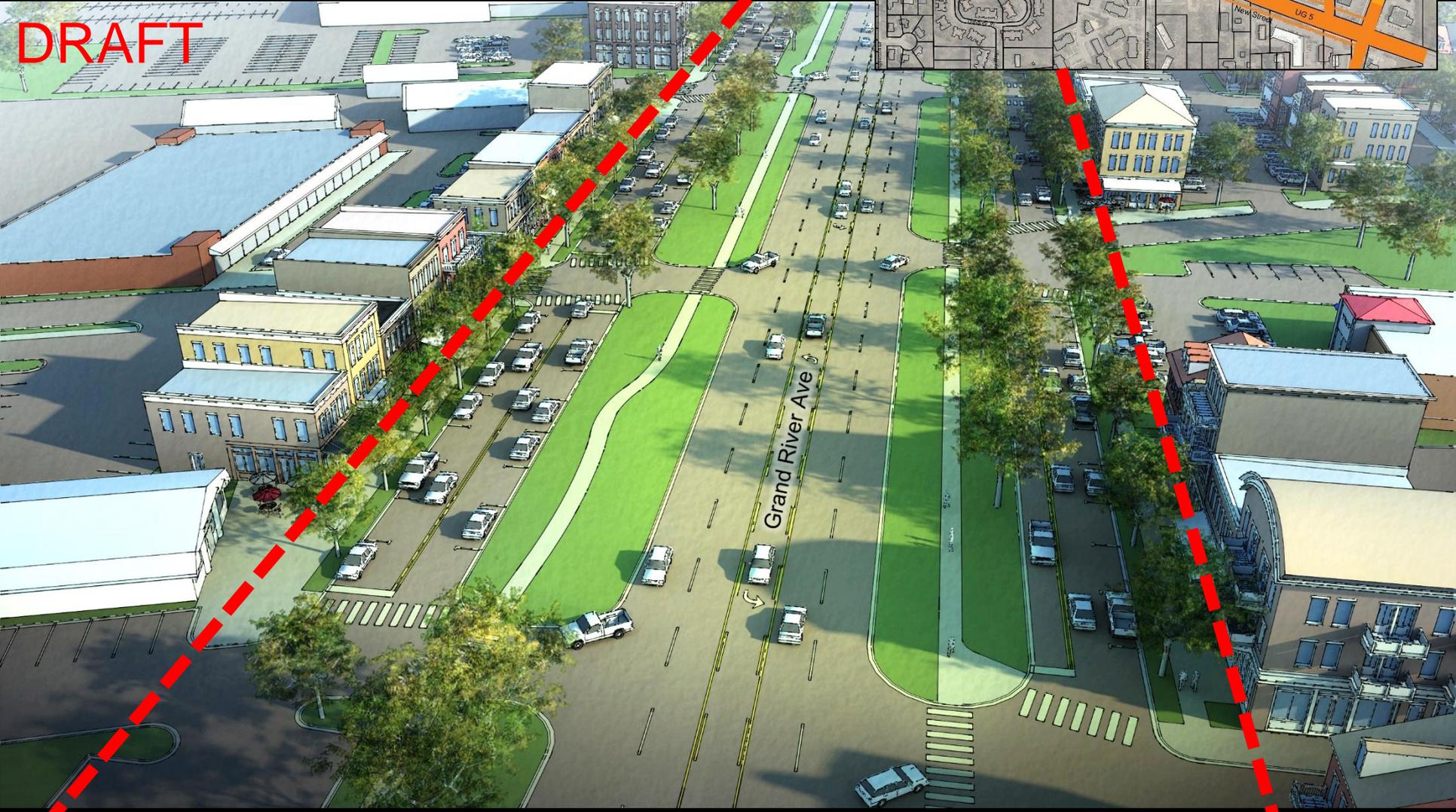
How do we account for piecemeal look of development if code is optional on Grand River Ave?

DRAFT

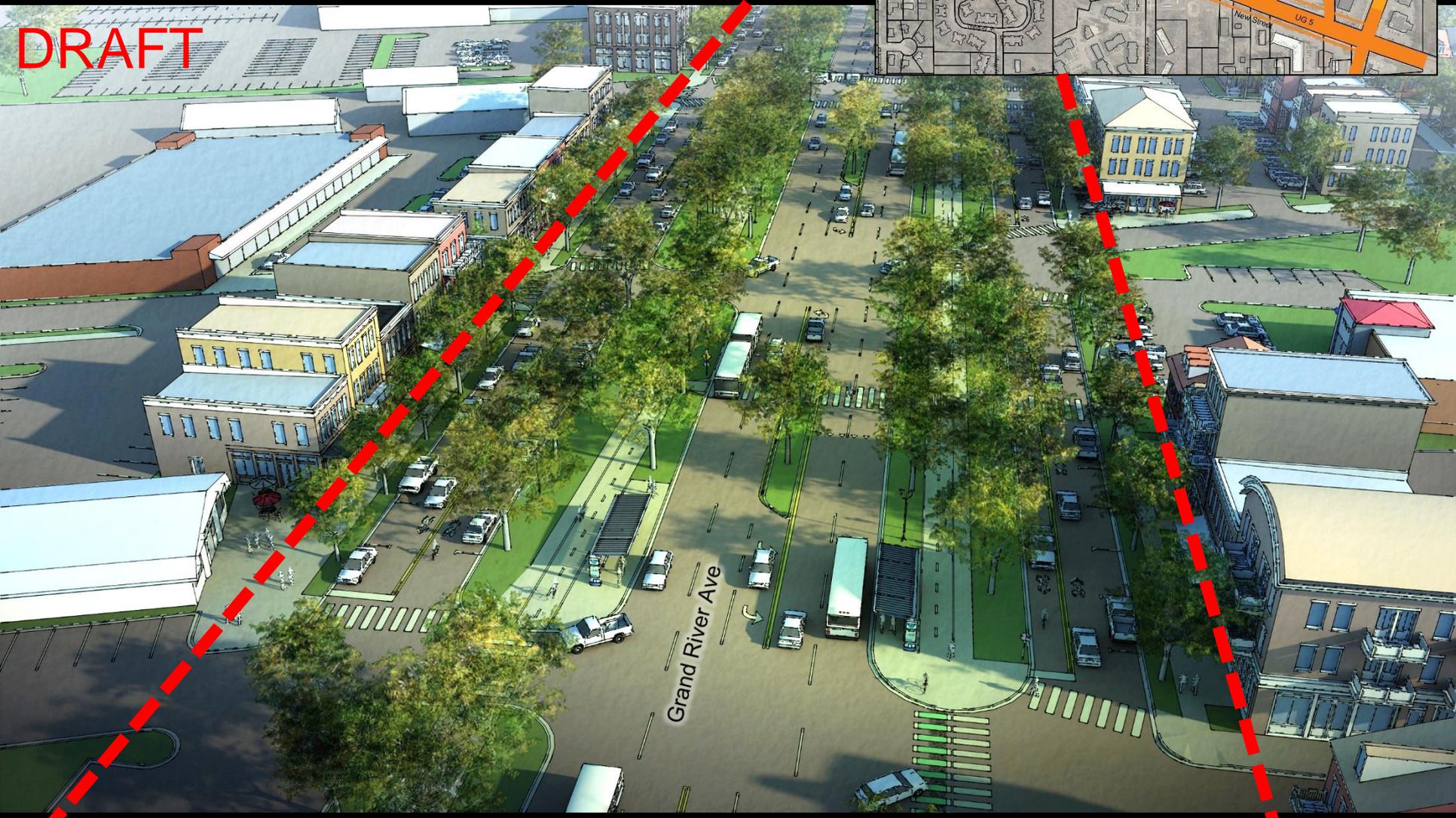


Existing Conditions along Grand River Avenue

DRAFT



Potential for change-over-time along Grand River Avenue



DRAFT



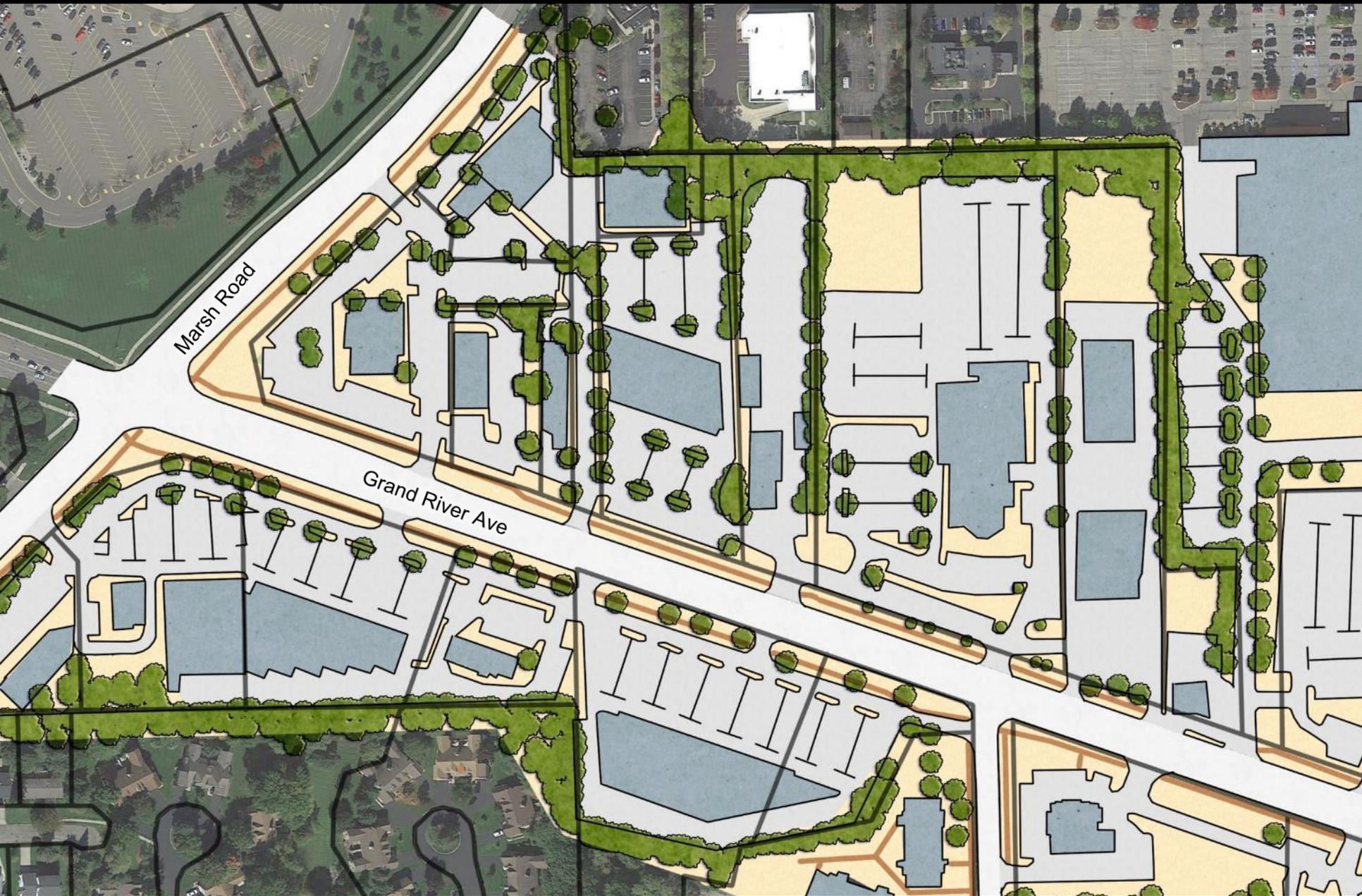
Potential for change-over-time along Grand River Avenue



Marsh Road

Grand River Ave

Existing Conditions along Grand River Avenue

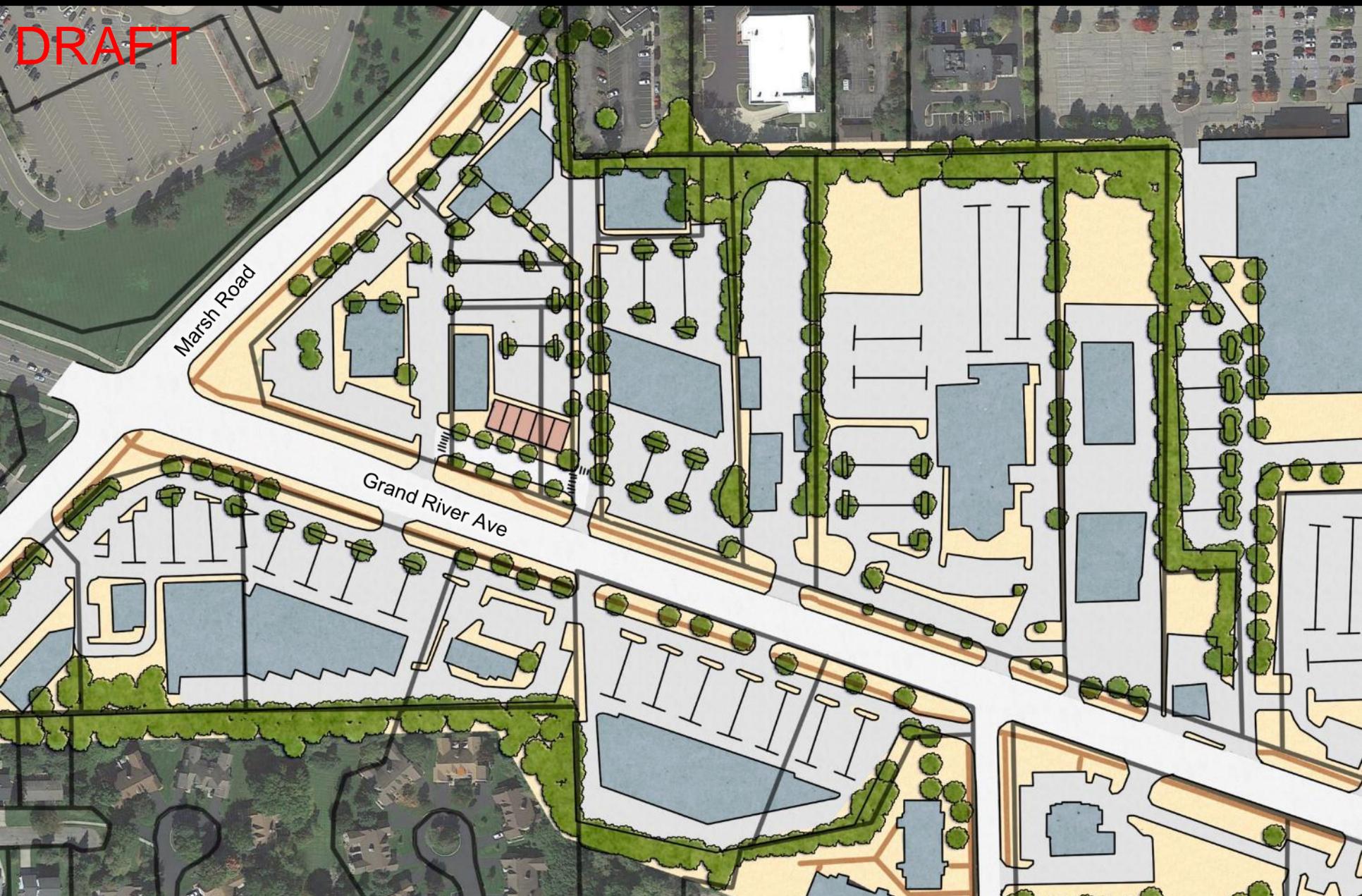


Marsh Road

Grand River Ave

Existing Conditions along Grand River Avenue

DRAFT

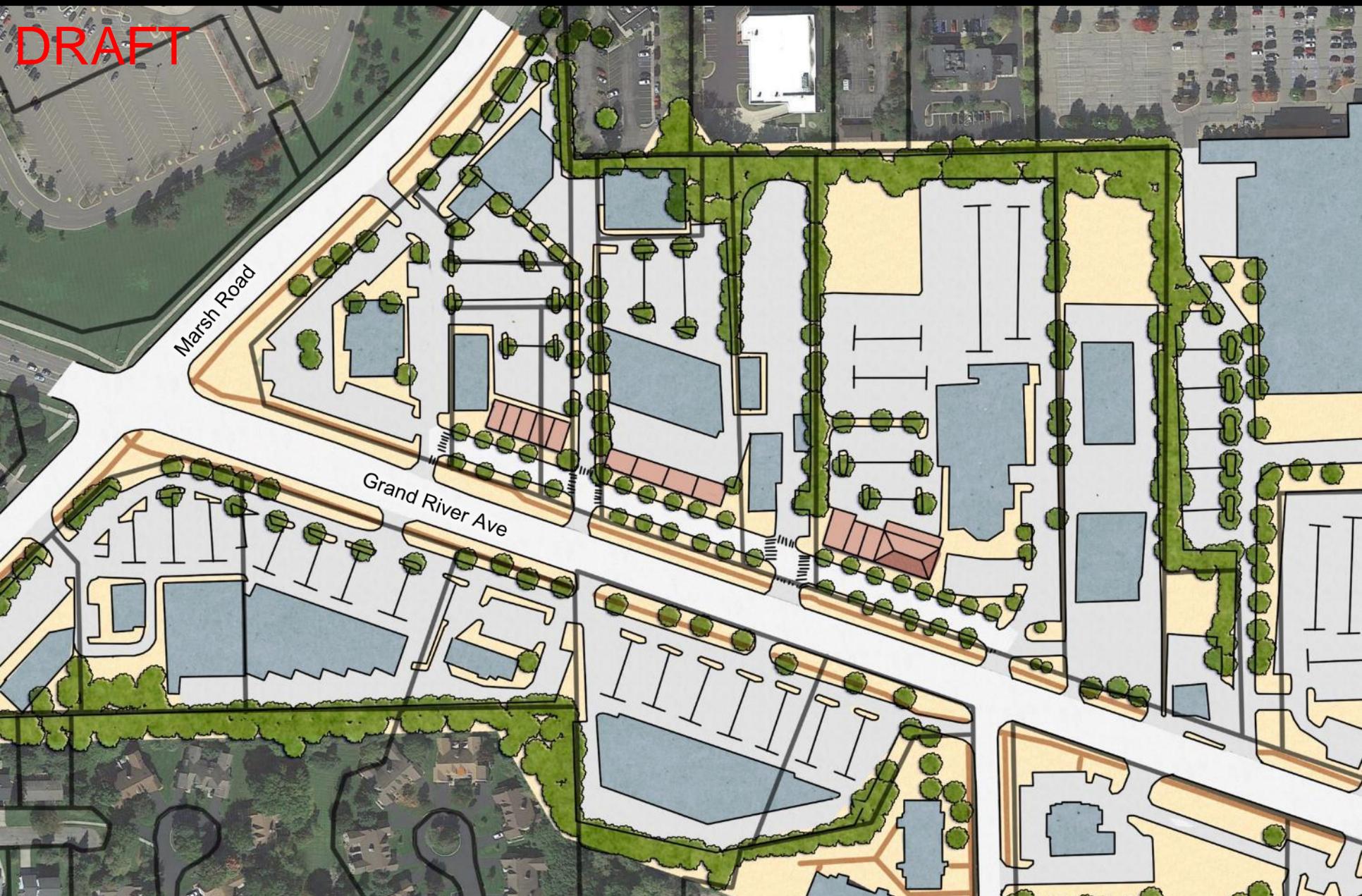


Marsh Road

Grand River Ave

Potential for change-over-time along Grand River Avenue

DRAFT

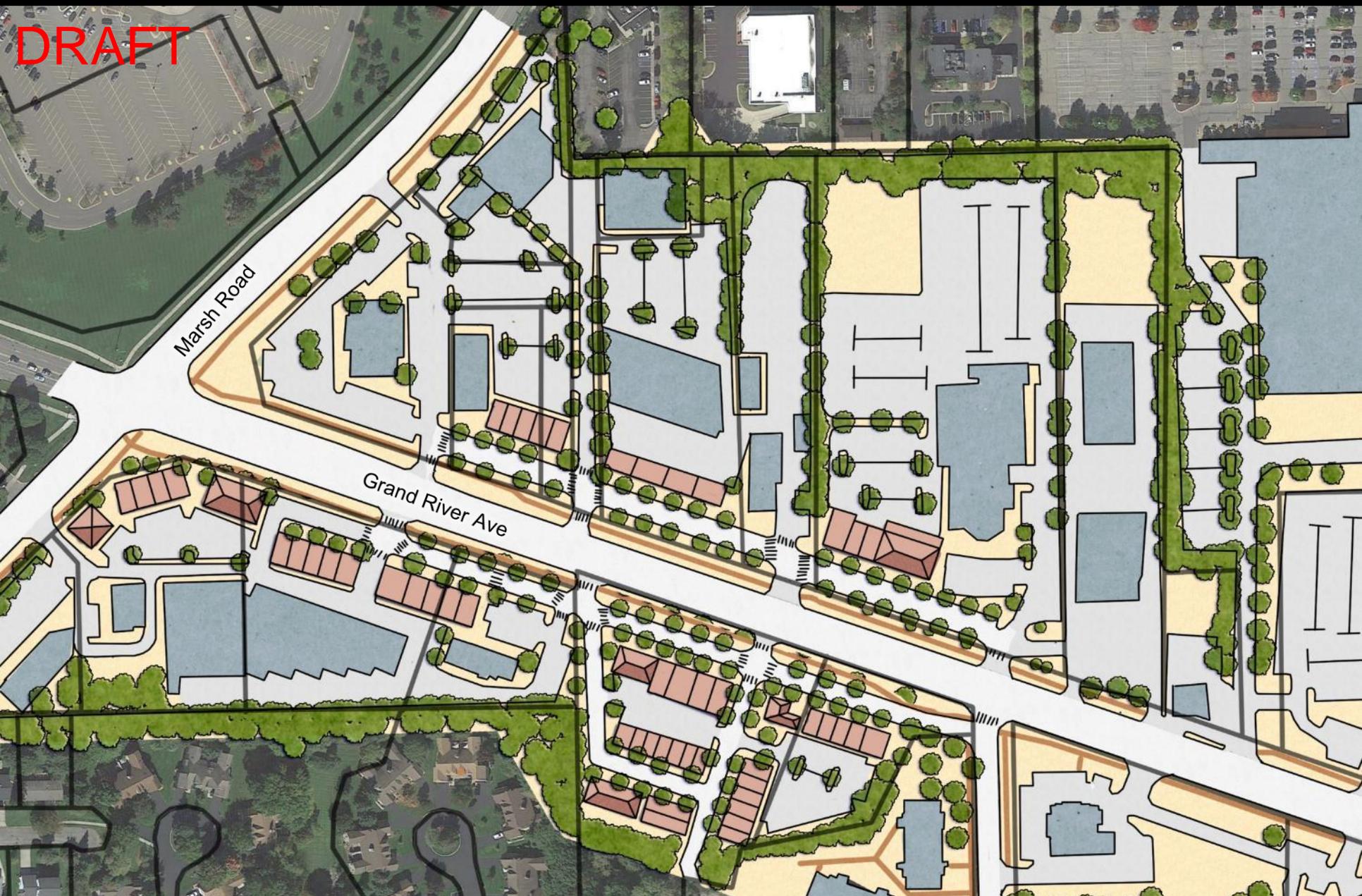


Marsh Road

Grand River Ave

Potential for change-over-time along Grand River Avenue

DRAFT



Marsh Road

Grand River Ave

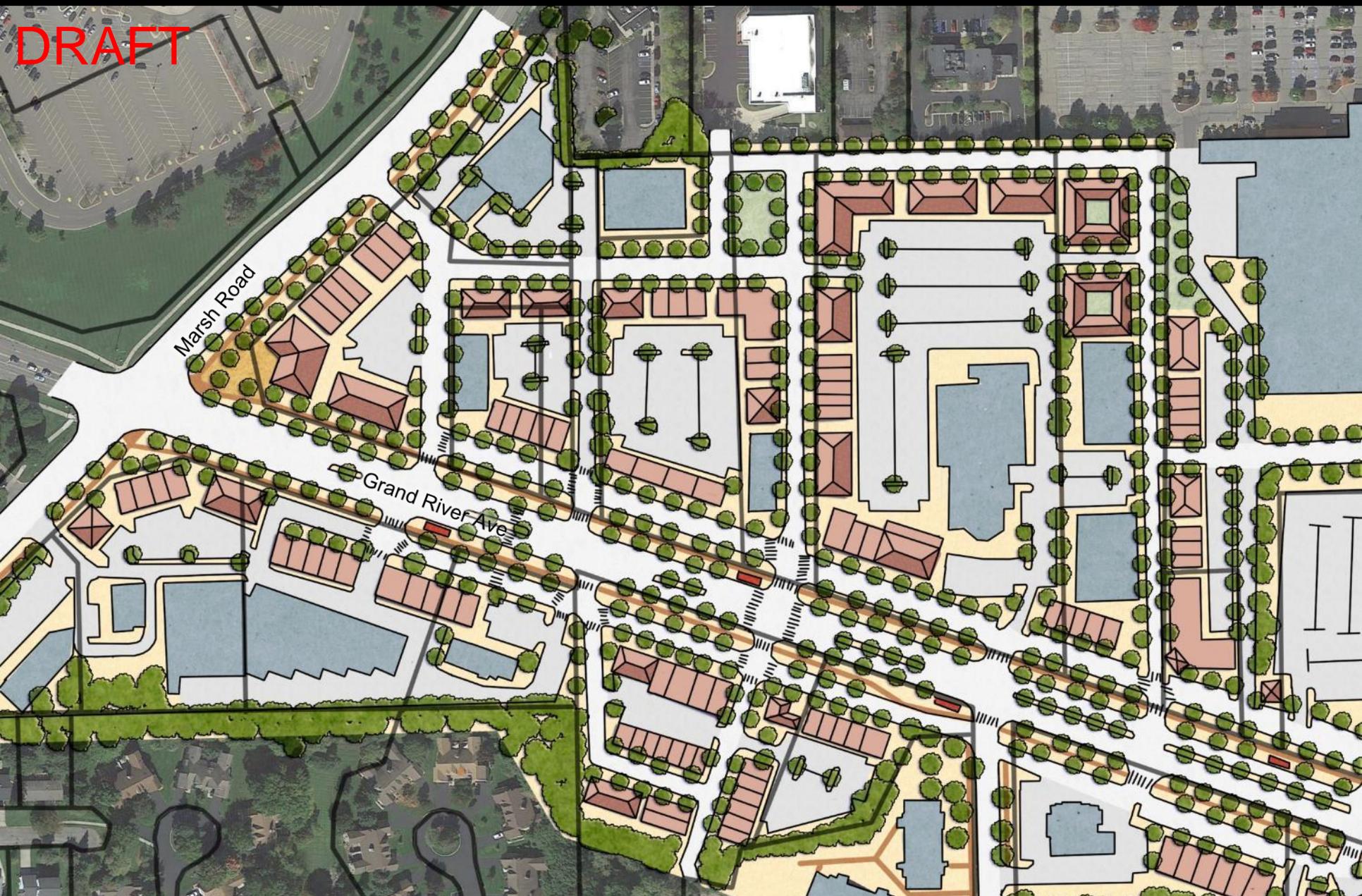
Potential for change-over-time along Grand River Avenue

DRAFT



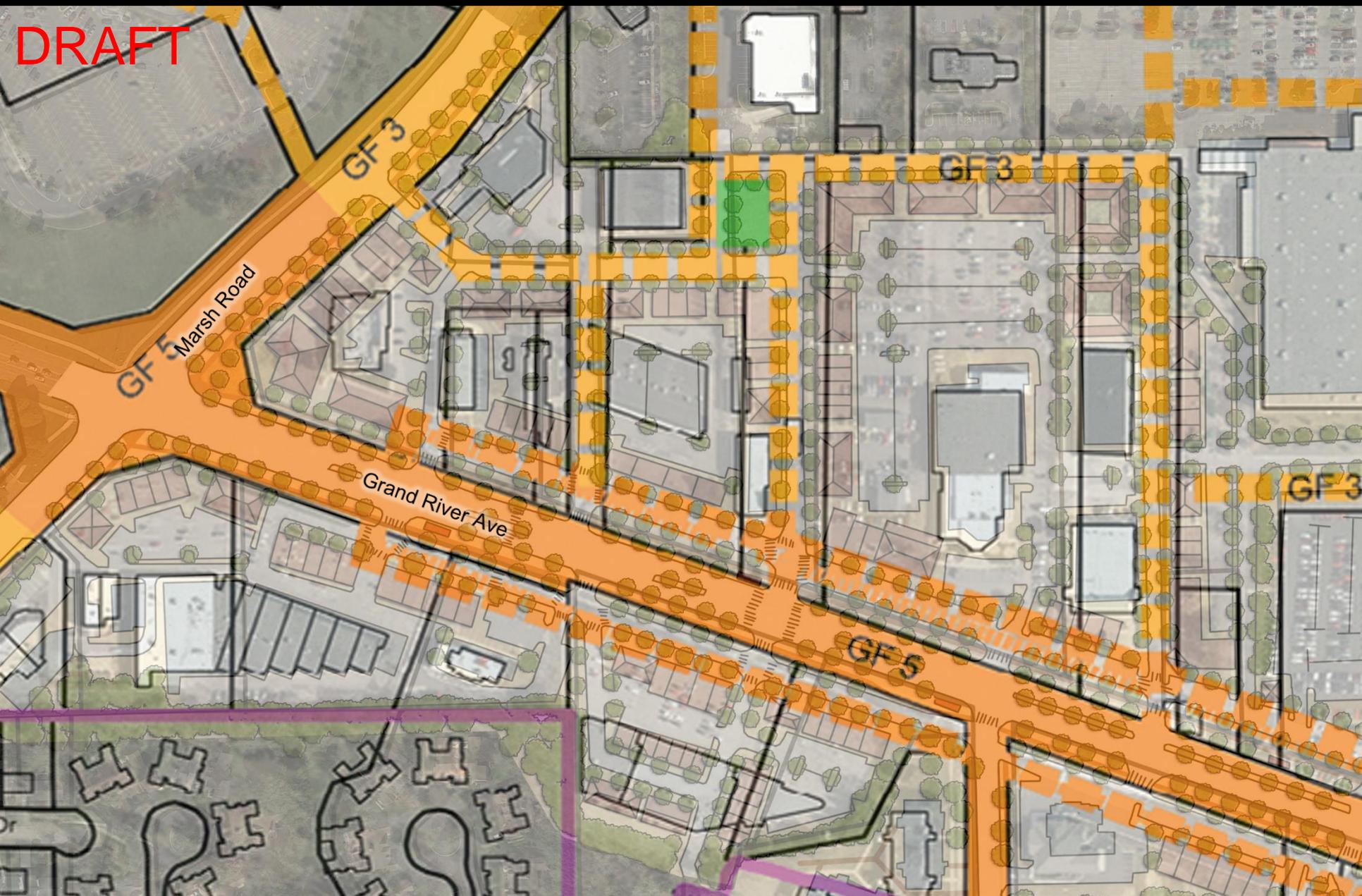
Potential for change-over-time along Grand River Avenue

DRAFT



Potential for change-over-time along Grand River Avenue

DRAFT



Potential for change-over-time along Grand River Avenue

your questions:

When/how are street improvements done?

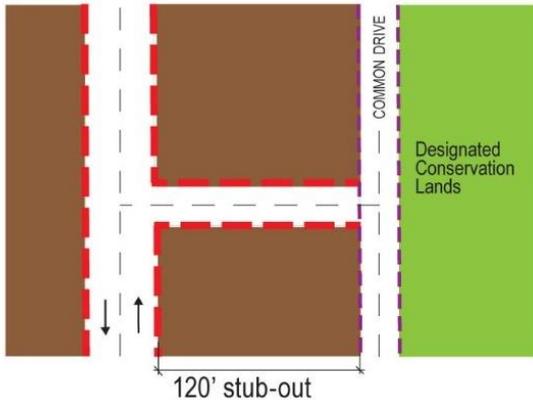


Figure 303.1.b.iii

2. New Streets

In addition to the street connectivity standards above, the following standards apply to those streets constructed after the adoption of this district.

- a. New streets designated on the REGULATING PLAN may or may not be immediately constructed.
 - (i) Any new streets that create building frontage on the parcel being developed shall be constructed at the time of development.
 - (ii) Public streets shall be placed into the system such that, when reasonable in the context of street maintenance or capital improvements plan, they can be constructed.
 - (iii) Private streets must be similarly scheduled for construction by the developer.
- b. If constructed within 50 feet of the designated center line location, these streets may be administratively approved, provided the resulting configuration meets these street configuration standards and that any other properties with frontage are not adversely effected.

your questions:

When/how are street improvements done?

Streetscape Elements Redevelopment Timing Matrix	Install new street trees (per 506.B)	Install street furniture (per 506.C)	Install/upgrade streetlights (per 506.C)	Install/upgrade sidewalks/shared pathways (to meet standard 506.C)	Construct pedestrian connections btwn sidewalks & buildings	Reconstruct Grand River frontage service drives (to meet standards in 509.B)	Install bike parking (per 506.D)	Close driveways that do not meet access standards (M-43 Access Mgmt. Plan)	Install / upgrade bus stops and transit amenities
Require for new site plans. Project funded by developer.	X			X	X	X	X	X	X
Require a financial guarantee or payment in lieu of changes. Funds go into a pool for future implementation when there is a larger project area.									
Project funded by the CIA or DDA. Funding assistance could be provided for closing driveways and redesign of frontage roads and access changes. If property owners participate, funding will be covered by the project. If not, property owners pay for it themselves later.	X	X	X	X			X	X	
Wait to include with ICRD/ MDOT project to reconstruct the road curb-to-curb. Since they are not likely to budget for all the desired improvements, this is the least preferred option.								X	
Funded by CATA.									X

Part 6: Architectural Standards

- Building Walls
- Roofs & Parapets
- Street Walls
- Windows & Doors
- Signage
- Lighting & Mechanical

Section 66-445
Olemiss Village and Grand River Avenue Form Districts

606. Windows and Doors

A. Purpose and Intent

The placement, type, and size of windows and doors on the FACADE largely establishes the scale and character of the STREET-SPACE. For retail buildings, windows allow interplay between the SHOPFRONT interiors and the STREET-SPACE. Commercial uses (especially restaurants and retail establishments) benefit from exposure to the passers-by and the STREET-SPACE benefits from the visual activity. For residences, windows foster the "eyes on the street" surveillance which provides for the security and safety for the area.

Windows should be divided by multiple panes of glass to provide a pedestrian scale.

B. Applicability

The standards in this section shall apply to any window or door that is CLEARLY VISIBLE FROM THE STREET-SPACE.

C. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only. Refer to the district standards on the following pages for the specific requirements of this section. The text and diagrams on the following pages have the power of law. Where photographs or statements on this page may be inconsistent with the text, the text shall prevail.



Door with transom and sidelight windows



Multi-paned storefront windows and glass doors



Storefront window



Grouped windows



Storefront Frontage



Grouped windows

Part 7: Parking Standards

- Minimum Requirements (p 71)
- Shared Parking (p 71)
- Location: on-site or within 800'; must be on same side of Grand River or within 1/8 mile of crosswalk (p 72)

Part 7: Parking Standards

2 Note to Staff: the consultant team recommends that a Parking Management Strategy be developed for the Okemos Village district, and potentially Grand River form District, enabling more progressive parking standards for a true pedestrian- and transit-oriented environment. Parking ratios could be reduced accordingly, such as the following:

Commercial, general retail, restaurants, taverns & bars, nightclubs, distilleries & brew pubs:

Option A:

2.0 spaces per 1,000 sq. ft. (minimum) and 3.0 spaces (maximum) for buildings with a gross floor area less than 25,000 sq. ft.

3.0 spaces per 1,000 sq. ft. (minimum) or 4.0 spaces (maximum) for buildings with a gross floor area over 25,000 sq. ft.

Option B:

No minimum and 3.0 spaces maximum for buildings with a gross floor area less than 15,000 sq. ft.

2.0 spaces per 1,000 sq. ft. (minimum) or 3.0 spaces (maximum) for buildings with a gross floor area over 15,000 sq. ft.*

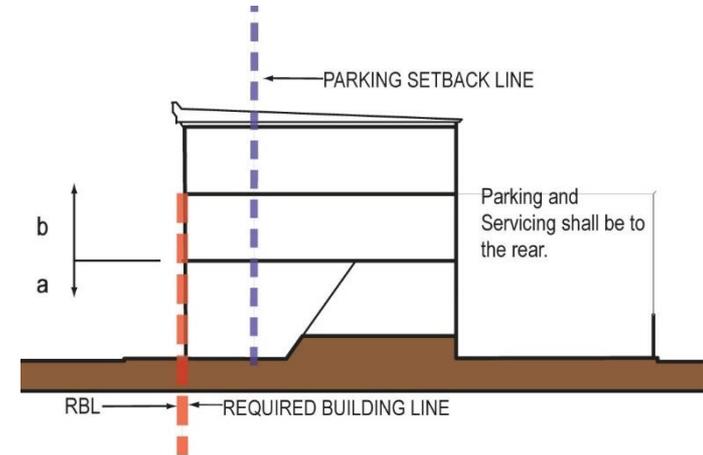
SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES

Part 8: Building Uses

USE CATEGORY		BUILDING FORM STANDARD						
		General Flex		Storefront		Townhouse/ Small Apt		
		Ground Story	Upper Story	Ground Story	Upper Story	Ground Story	Upper Story	
RESIDENTIAL	Household Living	■	■	■	■	■	■	Sec. 804.B.1-2
	Group Living		■		■	■	■	Section 86-654 of the Township's <i>Code of Ordinances</i>
COMMERCE	Office	■	■	■	■			Sec. 804.D.
	Overnight Lodging	■	■	■	■	■	■	Sec. 804.E.1-2
	Recreation/Entertainment	■	■	■	■			
	Vehicle Sales	■	■		■			Sec. 804.F.2
	Day Care	■	■		■	■	■	children: Section 86-403 (d)(1) adults: Section 86-403 (d)(2) of Township's <i>Code of Ordinances</i>
	Retail Sales & Service	■	■	■	■			Sec. 804.F.2, 5, 6
	Restaurant/Bar	■	■	■	■			Sec.804.F.1-4
	Vehicle service/ Gas Station	■						Sec. 804.G.
CIVIC	See Part 9. Definitions	■		■		■		Sec. 804.C.

Key: ■= Permitted ■= Additional Regulations Apply Blank Cell = Not Permitted



your questions:

In other communities, is governing body unwilling to give up authority on projects and delegate to staff?

your questions:

It would be helpful to take one or more hypothetical projects through the entire process under the draft code; from finding the property on the regulating plan all the way through the application and approval process.

other questions?

Thanks!



CORRIDOR DEVELOPMENT IMPLEMENTATION PLAN

Goal & Actions	Objectives	Lead	Additional Parties	Timeline	Budget	Deliverable	Complete?
1. Establish a Meridian Township Corridor Improvement Authority as defined by Act 280 of 2005	Initiate conversation with EDC board to make recommendation to Township Board for the creation of CIA	EDD	EDC	8/31/2016	n/a	Discussion & Recommendation	Yes
	Identify residents and business owners to recruit as members	EDC	CPD	10/31/2017	n/a	List of people to recruit	Yes
	Outreach to target member list to get list of those interested for presentation to Township Board	EDC	CPD	12/31/2017	n/a	List of interested parties	Yes
	Draft EDC letter officially recommending the formation of a CIA to the Township Board	EDC	CPD	1/31/2017	n/a	Memo and item on TB agenda	Yes
	Present the CIA applications for public service at the Township Board Meeting	EDC	CPD	2/7/2017	n/a	Presentation & appointments	Yes
	Host first CIA meeting to establish bylaws, officers, mission statement, goals & objectives, meeting schedule	CIA	EDC, CPD	3/31/2017	n/a	Completion and publishing of objectives listed	Yes
	Adopt CIA Work Plan - Goals and Objectives	CIA	EDD, CPD	7/31/2017	n/a	Adopted Work Plan	Yes
Goal & Actions	Objectives	Lead	Additional Parties	Timeline	Budget	Deliverable	Completed?
2. Engage with the Shaping the Avenue consulting team, educate the CIA on Form Based Code.							
	Staff provides Form Based Code introduction to the CIA	CIA	EDD, CPD	6/30/2017	n/a	Presentation	Yes
	Request presentation from Shaping the Avenue on Form Based Code	CIA	EDD, CPD	7/31/2017	n/a	Presentation	Yes
	Attend work sessions with Planning Commission and Township Board and advocate for revisions to draft code as agreed to by CIA	CIA	EDD, CPD, PC, TB	ongoing	n/a	Advocacy	
Work Plan Goals	Objectives	Lead	Additional Parties	Timeline	Budget	Deliverable	Completed?
1. Promote a consistent and pleasing aesthetic style for buildings, grounds, and public right-of-way. Identify and coordinate streetscape improvements needed throughout the corridor to achieve the desired traditional look for Township. This will include items such as brick pavers, street trees, lights, benches, waste receptacles, etc.	Engage with the Township Communications Dept for information about updated street signs, get pricing and renderings, present to the Township Board for budget approval	EDD	CD, CIA, TB				
	Work with MDOT on their timeline for overhaul of Grand River Avenue. Confirm what the Township can suggest and when they need to be presented by	EDD	PWE, CIA, PC, TB, EDC				
	Solicit feedback from stakeholders regarding wishlist ideas for Corridor Improvements during MDOT work	EDD	CIA, EDC, TC, EC, PC, TB, Residents, Business Owners				
2. Provide a common vision for corridor businesses. This will be accomplished through SWOT analysis and roundtable discussions in the community. Data will be collected and compiled to gather community vision for the corridor. By gaining better insight of how the collective, "common vision" is defined in the community, the CIA can provide a better framework of design for corridor businesses.							

<p>3. Strive to increase safety and accessibility for pedestrians & cyclists. This will be accomplished through creation of a path and sidewalk system that provides safe refuge for non-motorists while traveling along the corridor and between buildings. Review the Township pathway plan that identifies the most appropriate treatment for each section of the corridor. This pathway plan will include more precise cost estimates so the Township can seek grants and alternate ways to assist property owners.</p>	<p>Confirm what projects have already been approved and funded, compare with the research from the Transportation Commission, Public Works and Parks Department have gathered and help advocate for improvements along the corridor</p>	<p>EDD</p>	<p>CIA, PWE, PD, TC</p>	<p>8/31/2018</p>	<p>n/a</p>	<p>List of projects</p>	
<p>4. Promote smart growth on the corridor and include infill development while discouraging sprawl. Create a long-range development strategy that focuses on solid growth, expanded employment and stable tax revenues. Institute an early warning system for business closings, relocations and layoffs. Objective: Identify corridor companies ripe for expansion. Remove barriers to their growth.</p>	<p>Review all corridor related projects, advocate for infill development and prevent sprawling development projects</p>	<p>CIA</p>	<p>CPD, EDC, PC, TB</p>	<p>Ongoing</p>	<p>n/a</p>	<p>Infill development, less sprawl</p>	
	<p>Create a Corridor business listing with contact information</p>	<p>EDD</p>	<p>EA, CIA, EDC</p>	<p>8/31/2018</p>	<p>n/a</p>	<p>Business listing</p>	
	<p>Develop an on-line survey for these businesses to gain an overall understanding of growth plans, struggles, ideas for business assistance</p>	<p>EDD</p>	<p>EA, CIA, EDC</p>	<p>10/31/2018</p>	<p>n/a</p>	<p>Survey results</p>	
<p>5. Improve recognition of corridor entry ways and signage. Designate attractive corridor entryways with landscaping, signage, lighting and architectural features like brick knee walls. A sample design is under review by the Meridian Township Sign Branding Committee.</p>	<p>See tasks under Work Plan Goal #1. Advocate for funding for the project via the Sign Branding Committee</p>	<p>CIA</p>	<p>CPD, CD, TC, EDC, PC, TB</p>	<p>10/31/2018</p>	<p>n/a</p>	<p>Project included in the TB Budget</p>	
<p>6. Encourage business recruitment and expansion. Coordinate with the Meridian Township EDC to supplement and review business retention methods and results. Contribute comments and observations to relevant staff and committees.</p>	<p>See tasks under Work Plan Goal #4. Use this data to develop a business outreach, retention, expansion and attraction plan</p>	<p>EDD</p>	<p>DDA, CIA, EDC</p>	<p>12/31/2018</p>	<p>n/a</p>	<p>Approved business retention, expansion and attraction program</p>	
<p>7. Improve the Township's reputation within the business and residential community and promote communication. Develop a joint marketing strategy with involvement of the business community that brings together resources for advertising, special events and public relations. To effectively market the corridor, a branding effort must be undertaken that creates a clear, positive image to potential shoppers, visitors, investors, etc. This image must then be promoted over multiple media platforms in a comprehensive, consistent manner to educate and inform the public. Prepare marketing materials that can be presented to developers, retailers and real estate firms promoting the Corridor. This may include brochures, exhibits, trade area data, web site materials, social media activities, etc.</p>							
<p>8. Strive to increase business relationships with regional partners. Encourage corridor business owners to join regional partners, such as the Lansing Chamber of Commerce and the Meridian Area Business Association. Attend regional partner networking events, such as monthly MABA meetings.</p>							

TB = Township Board, PC = Planning Commission, EDD = Economic Development Director, CPD = Community Planning & Development, PD = Parks Department, PWE = Public Works & Engineering, PF = Police & Fire Departments, EDC = Economic Development Corporation, DDA = Downtown Development Authority, CIA = Corridor Improvement Authority, TC = Transportation Commission, EC = Environmental Commission, CD = Communications Department, PAC = Parks Commission, BRA = Brownfield Redevelopment Authority, LPAB = Land Preservation Advisory Board, Mteam = Township Directors for Various Departments
EA = Executive Assistant, TM = Township Manager, LEAP = Lansing Economic Area Partnership, CCC = Capitol Community Connections, MABA = Meridian Area Businesses Association, TCRPC = Tri County Regional Planning Commission