

SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES

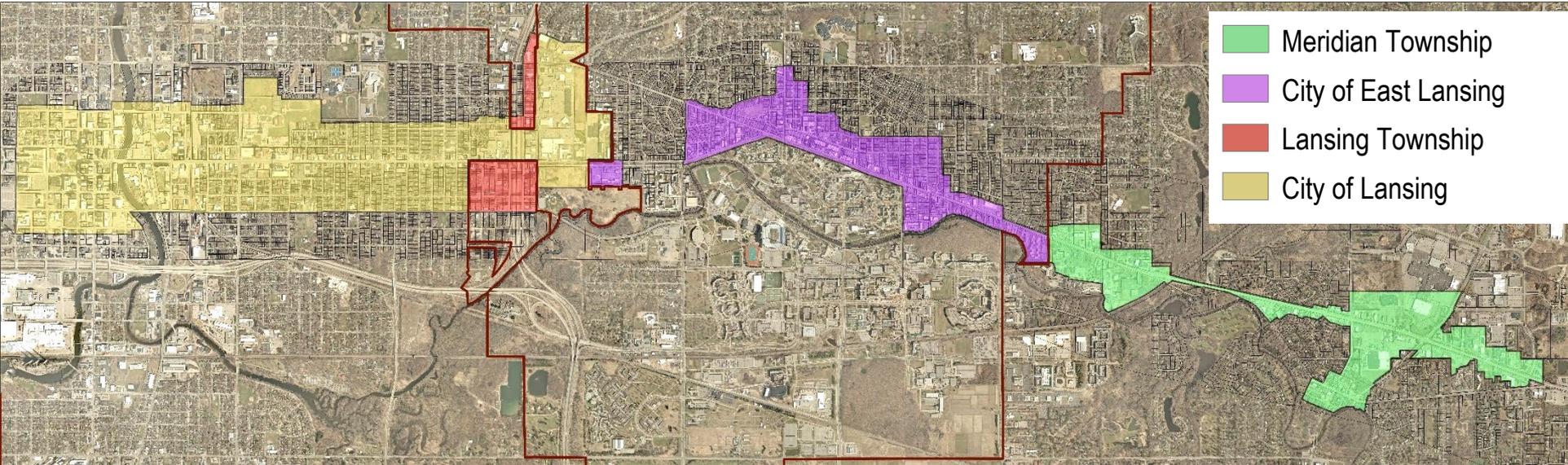
Meridian Township
Planning Commission Work Session
April 30, 2018

www.shapingtheavenue.com

Study Area

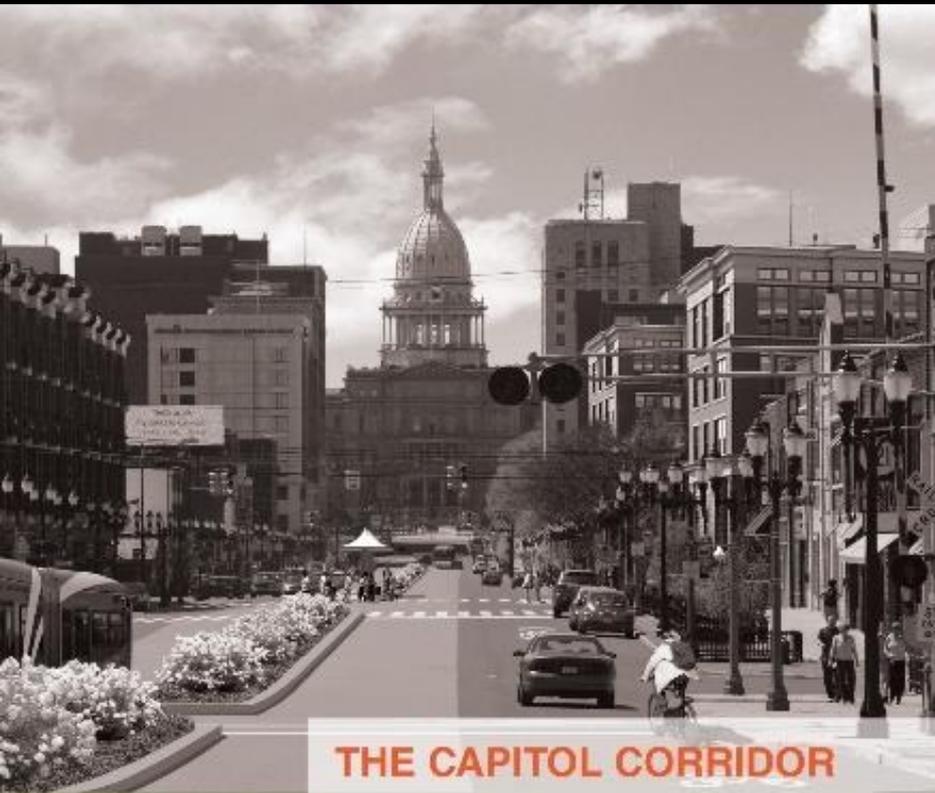
SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES



what is Shaping the Avenue?

... putting the guidelines in place that will lead the Michigan/Grand River Avenue corridor toward its full potential to support residents, businesses, cars, bikes, pedestrians and transit users.



THE CAPITOL CORRIDOR

a regional vision for
Michigan Avenue / Grand River Avenue



Building on the
visioning completed
for the
Capitol Corridor

what are form-based codes?

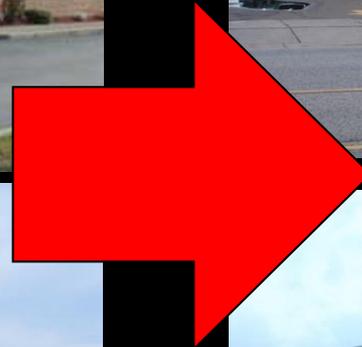
A method of regulating development to achieve a specific urban form. Form-Based Codes create a predictable public realm by controlling physical form primarily, and land uses secondarily, through city or township regulations.

Results from different zoning approaches

Conventional



Form-Based



Shaping commercial corridors



Front of bldg. faces the street/sidewalk
Parking lot is to the rear



Rear of bldg. faces the street/sidewalk
Parking lot is to the front

Open House (Sept 2017)

SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES



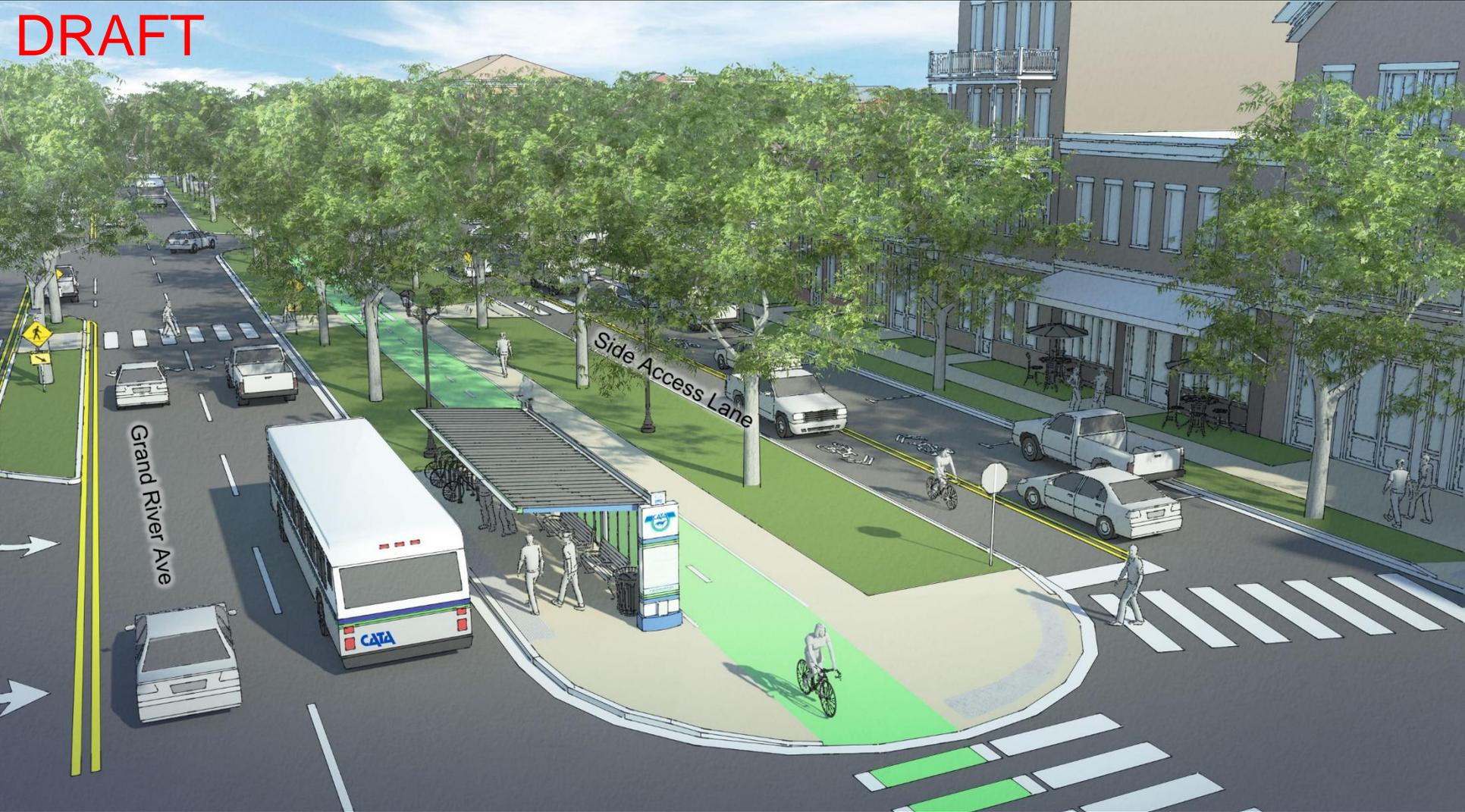
Sept 12, 2017: Young Pros Event

Sept 13, 2017: City of Lansing

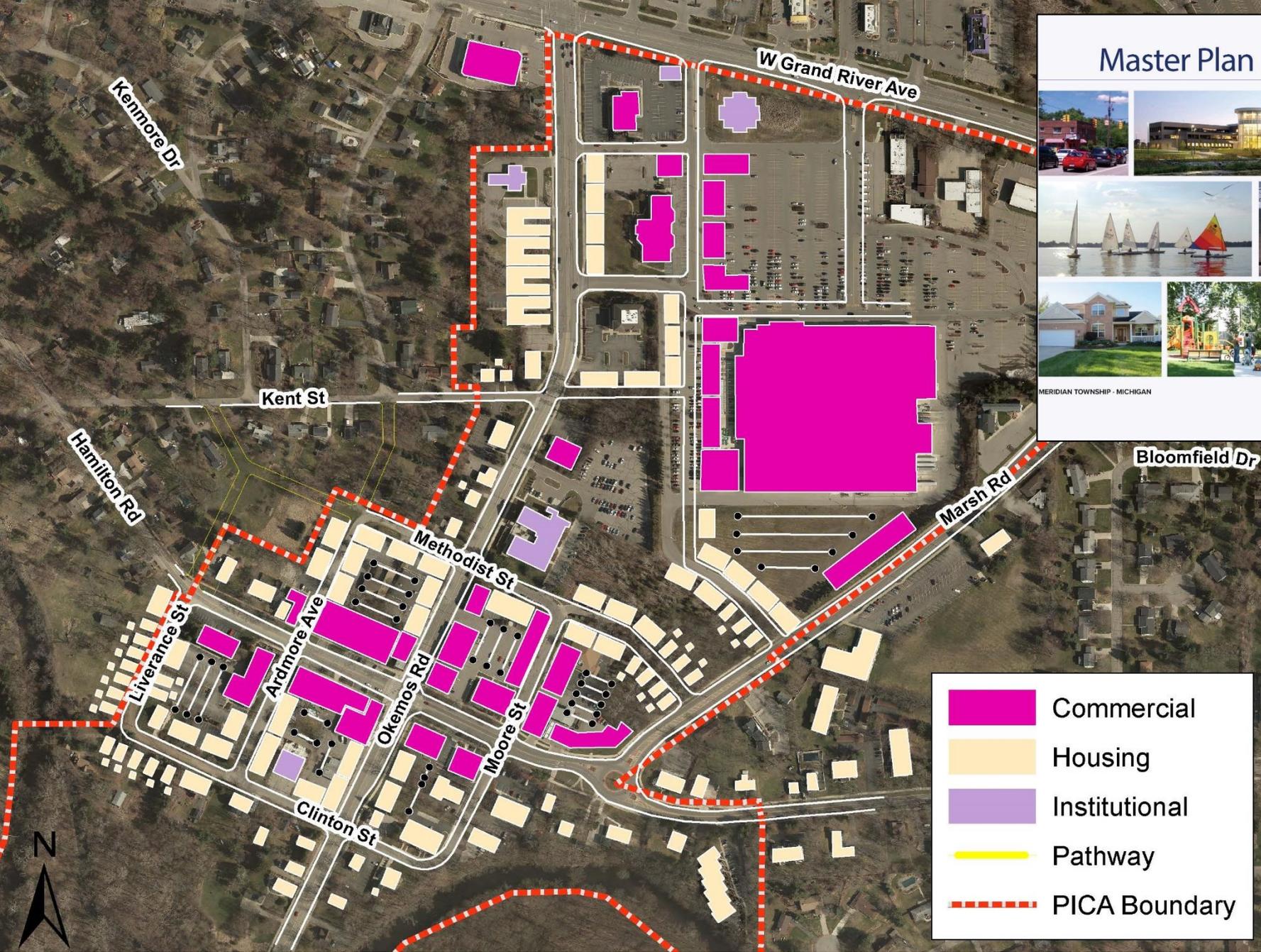
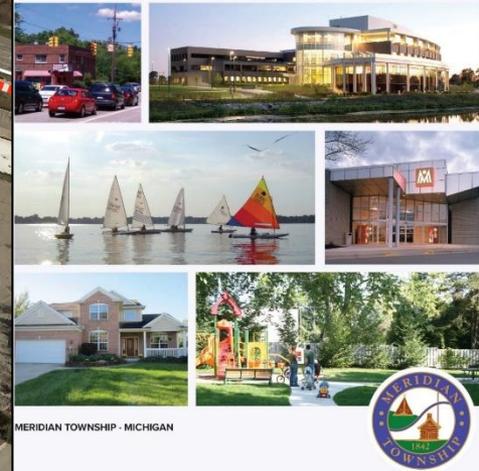
Sept 14, 2017: Meridian Township

Sept 18, 2017: East Lansing

DRAFT



Shaping the Avenue in Meridian Township



- Commercial
- Housing
- Institutional
- Pathway
- PICA Boundary

Okemos PICA. Located along Okemos Road a short distance south of Grand River Avenue, this once-thriving historic activity center is utilized as a vehicular cut-through.

Okemos Today. The street grid is desirable in scale and has the potential to support mixed use infrastructure of a walkable urban form. In its current form, Okemos lacks true connection to the active commercial corridor just minutes to the north. The center is missing an identity and purpose, even though visibility and accessibility are high.

A Bustling Downtown Tomorrow. The Okemos core will embody traditional urban living, with emphasis on creating spaces for living, working, and recreation within the neighborhood.

New development in Okemos will take advantage of its location to provide residents great access to regional transportation systems and draw in visitors with unique retail and service amenities. First floor storefronts and upper-level apartments and condos will provide an eclectic mix of users that will take advantage of plazas and street cafes to create an active streetscape.



Potential Transformation of Okemos Village

3

GOALS AND OBJECTIVES

Enhance the Viability of Township Businesses

A Upgrade commercial areas.

STRATEGIES:

1. Participate in regional efforts to redevelop the Grand River corridor, such as the Grand River Corridor Improvement Authority, and incorporate results where appropriate.
2. Improve and enhance the Haslett Road commercial area while maintaining its traditional character.
3. Encourage redevelopment of the downtown Okemos area using the Downtown Development Authority (DDA) Integrated Plan as a guide.
4. Evaluate opportunities for walkable urban design in commercial areas, including wayfinding solutions that improve usability.
5. Encourage the use of the commercial planned unit development ordinance.
6. Identify specific areas where the form-based code technique may be appropriately applied.

B Develop the Township's mixed use core into a viable and vibrant district.

STRATEGIES:

1. Explore the viability of upper floor(s) residences in the mixed use core.
2. Continue to engage the public in deciding the direction for development in the mixed use core.
3. Explore options for public-private partnerships to revitalize the Township's mixed use core.

C Improve the attractiveness of the Township's entrances and transportation corridors.

STRATEGIES:

1. Investigate grant-based funding opportunities to improve landscaping along major thoroughfares.
2. Create well-designed entrances along the main roads entering the community.
3. Promote opportunities for public art.

D Promote economic development.

STRATEGIES:

1. Communicate with and utilize the resources of the Economic Development

6

GOALS AND OBJECTIVES

Provide and Support an Efficient, Safe, and Environmentally Sensitive Multi-modal Transportation Network

A Evaluate the existing transportation network and identify problems, including traffic congestion, safety, environment, and aesthetics, which can be solved at acceptable cost.

STRATEGIES:

1. Cooperate with county, regional, and state entities to develop strategies to improve traffic flow.
2. Consider zoning and other design strategies such as Complete Streets as effective means of ensuring safe and efficient travel for all modes of transportation.

B Maintain local roads.

STRATEGIES:

1. Support improvements to local streets when a special assessment district request has been received from local residents.
2. Review reconstruction and expansion of local non-subdivision streets through the Capital Improvements Program process, when 50 percent of the improvement's funding will be paid for by the Township or its residents through special assessment districts.

C Support the Township's Complete Streets Ordinance.

STRATEGIES:

1. Share the Township's vision and work with transportation and road authorities.
2. Review current policies to identify inconsistencies and impediments to implementing the Complete Streets ordinance.
3. Conduct planning, regulatory, and physical audits of current street designs as needed and financially feasible.
4. Coordinate the Pedestrian/Bicycle Pathway Plan with Safe Routes to School policies and other Township plans.

D Expand and improve the Pedestrian/Bicycle Pathway Plan.

STRATEGIES:

1. Continue to implement the requirement for sidewalks as determined necessary.
2. Continue to implement the pedestrian/bicycle pathway system through site plan review.
3. Continue to support the pedestrian/bicycle pathway millage for construction and maintenance of the Township's pedestrian/bicycle pathway system.
4. Investigate alternative surfacing materials to reduce the amount of impervious surface materials used in the construction of the pedestrian/bicycle system.
5. Continue to investigate strategies for safe and efficient travel for bicyclists and pedestrians.
6. Coordinate the Pedestrian/Bicycle Pathway Plan with parks, schools, preserves, Greenspace Plan, neighborhoods, and other destinations.

E Encourage the use of public transportation.

STRATEGIES:

1. Encourage and promote public transportation service to any new developments and in conjunction with redevelopment via site plan review.
2. Institute design standards for site plan review to ensure public transportation access to all segments of the population but particularly those with physical disabilities.

F Coordinate Meridian Township's transportation planning with regional planning efforts administered by the Tri-County Regional Planning Commission and recorded in the 2035 Regional Transportation Plan.

STRATEGIES:

1. Coordinate transportation planning and road improvements for all non-local streets with the region's long-range plan.
2. Advocate and promote the Township's needs and interests in transportation improvements with transportation and road authorities.

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GOALS AND OBJECTIVES

Promote efficient and sustainable growth practices

A Implement sustainable energy and environmental practices throughout the Township by utilizing the most current best practices.

STRATEGIES:

1. Budget funds to have Township buildings assessed for energy efficiency a minimum of once every five years.
2. For Meridian Township procurement purposes, when equivalent products or services are available, preference should be given to the more sustainable services or products.
3. Encourage electric car charging stations.
4. Establish a minimum level of LEED certification or the equivalent for all projects undertaken by the Township.
5. Establish ordinances providing incentives for development that emphasize redevelopment, infill development, alternative energy, and building material reuse.
6. Increase the use of alternative transportation modes for commuting and recreation by encouraging the use of mixed use planned unit developments and implementing the Greenspace Plan.
7. Explore methods, through ordinances or by providing incentives for existing businesses, to upgrade their parking lots and landscaping to increase tree cover and shade, to be energy efficient, and environmentally friendly.
8. Explore incentives for new or redeveloped projects to be LEED certified or equivalent.

DRAFT



Existing Conditions along Grand River Avenue, Meridian Township

DRAFT



Potential for change-over-time along Grand River Avenue, Meridian Township



Potential for change-over-time along Grand River Avenue, Meridian Township

DRAFT



Potential for change-over-time along Grand River Avenue, Meridian Township



DRAFT

Potential for change-over-time along Grand River Avenue, Meridian Township

Draft Code Elements:

1. Purpose and Intent
2. Administration and Applicability
3. Regulating Plans
4. Building Form Standards
5. Public Realm Standards
6. Architectural Standards
7. Parking Standards
8. Building Uses
9. Definitions

Part 1: Purpose & Intent

- Create more walkable pedestrian-friendly and transit-supportive mixed-use environments in the Okemos Village center and along the Grand River Avenue corridor
- Places a primary emphasis on placemaking (physical form and character), and has a secondary focus on land uses

Part 2: Administration

- Grand River Avenue Form District
 - **OPTIONAL**: if opt in, must follow standards
- Okemos Village Form District
 - **MANDATORY**: thresholds for compliance (p 5)
- Establishes process and authority for review / approval

your questions:

What are the benefits to opting in to the form-based code?

- Expedited approvals: fewer special uses and public hearings for projects that comply
- Increased height/density, reduced parking
- Allow greater mixture of uses
- Recommended: Fewer studies required (e.g. traffic impacts) for mixed use
- Recommended: Amend C District, restrict uses / add MU-PUD design standards

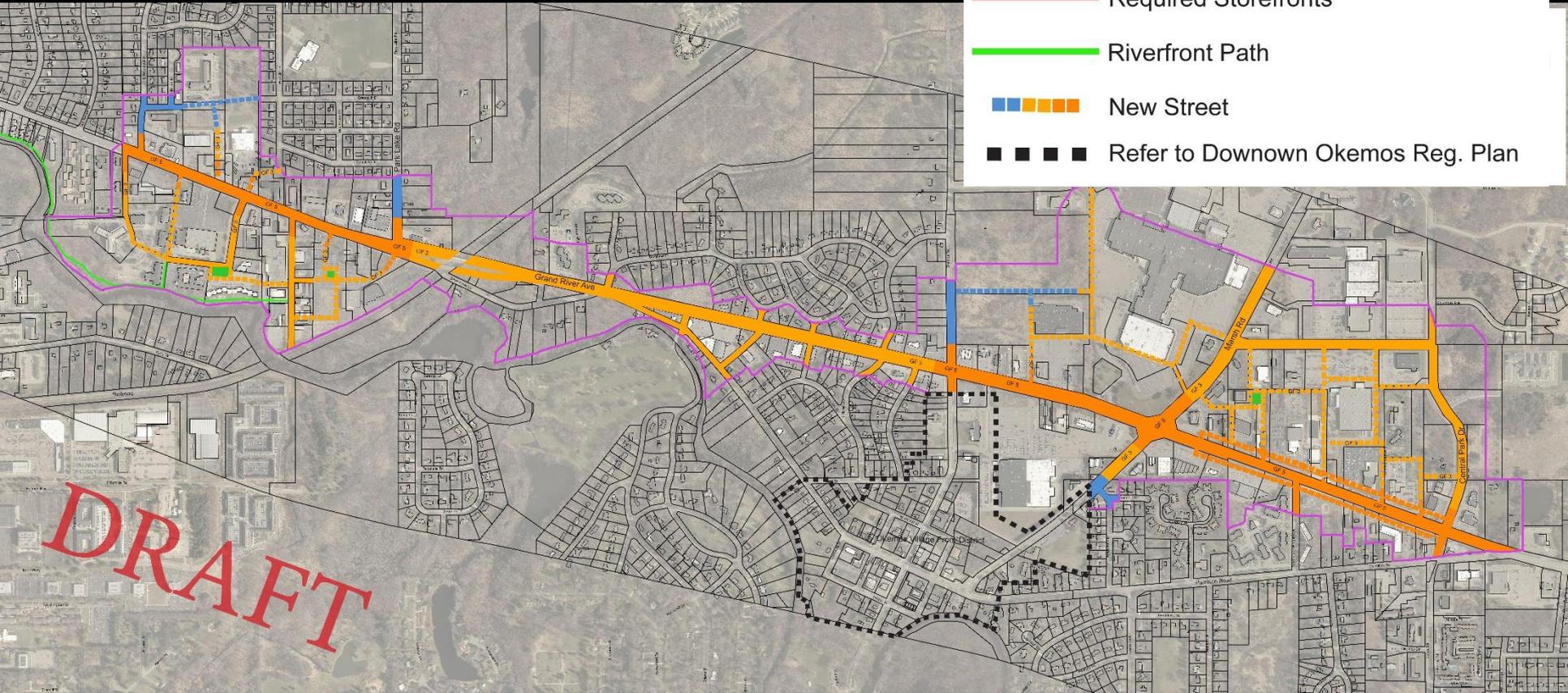
Draft Code Elements:

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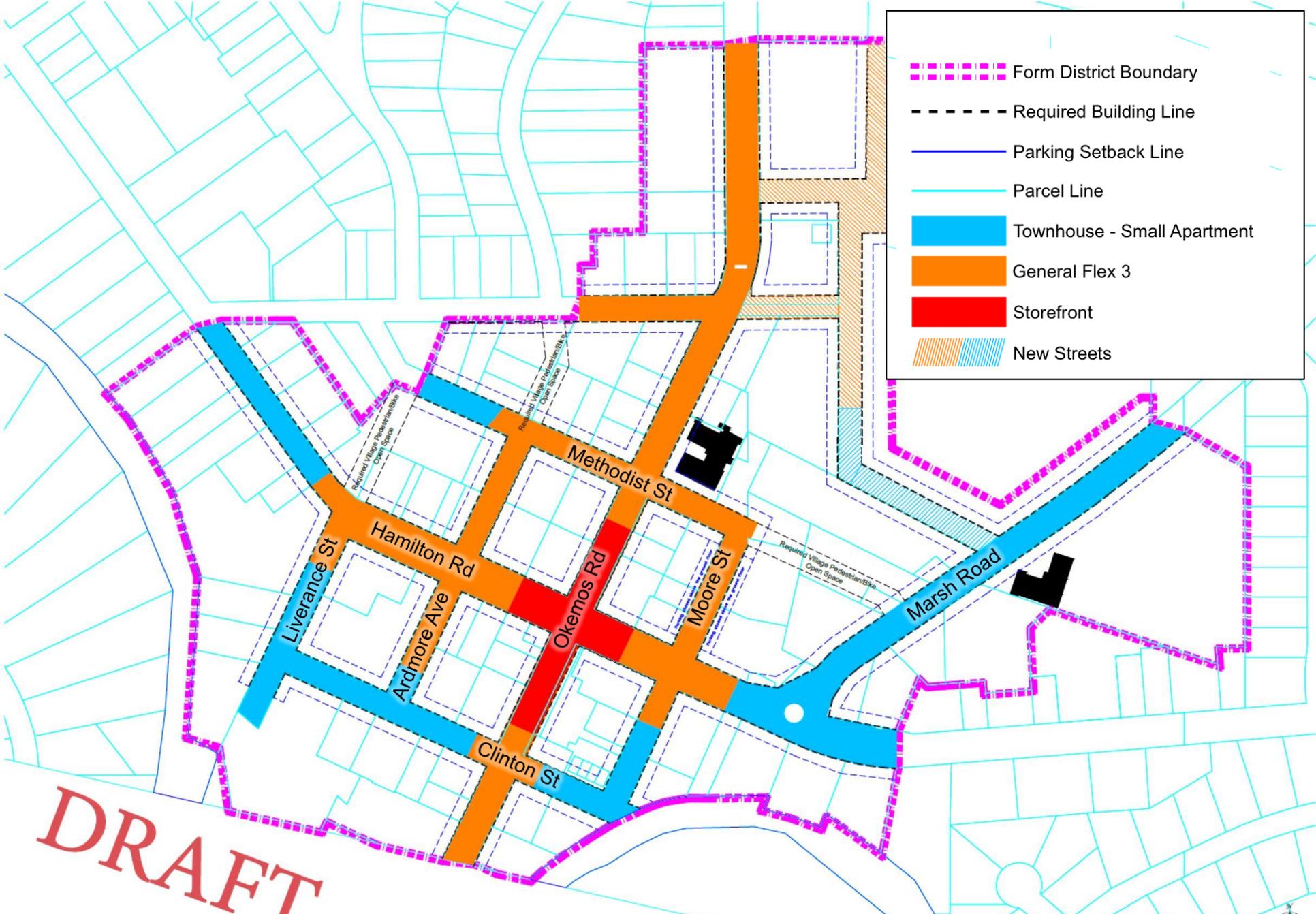
Parts 3 to 6
define form &
character

Part 3: Regulating Plans

- Required Open Space
- Townhouse - Small Apt.
- General Flex 3 (Number = Max Height)
- General Flex 5
- District Boundary
- Required Storefronts
- Riverfront Path
- New Street
- Refer to Downtown Okemos Reg. Plan

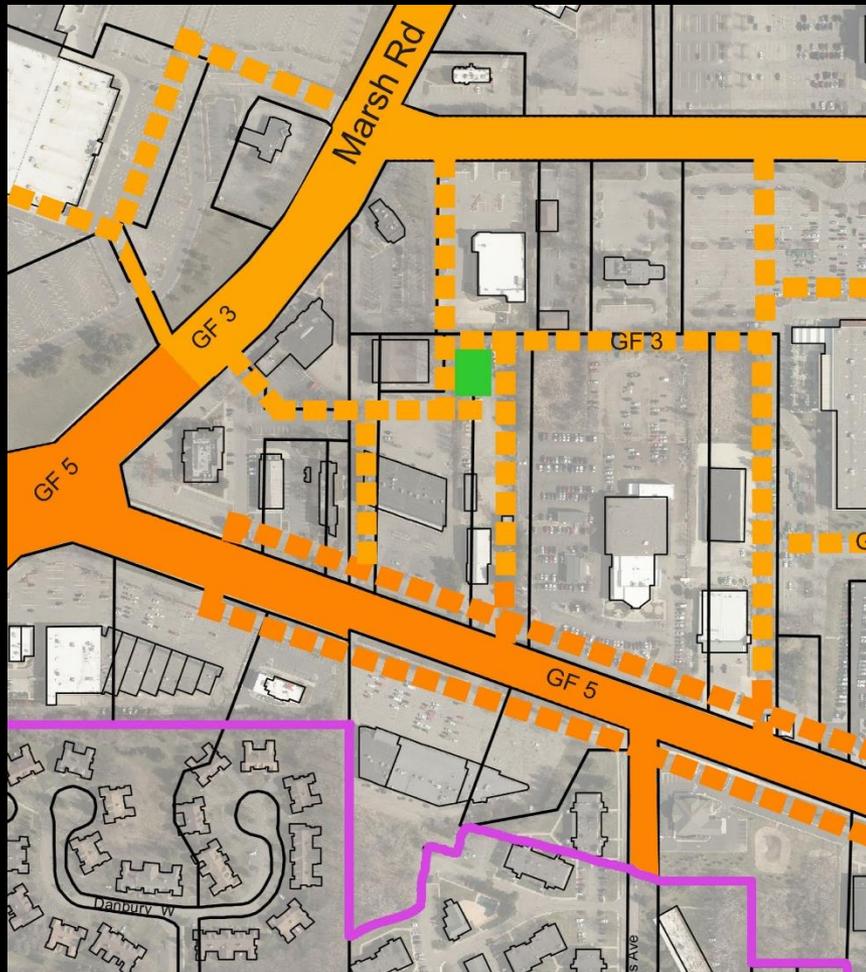


Grand River Avenue Form District Regulating Plan



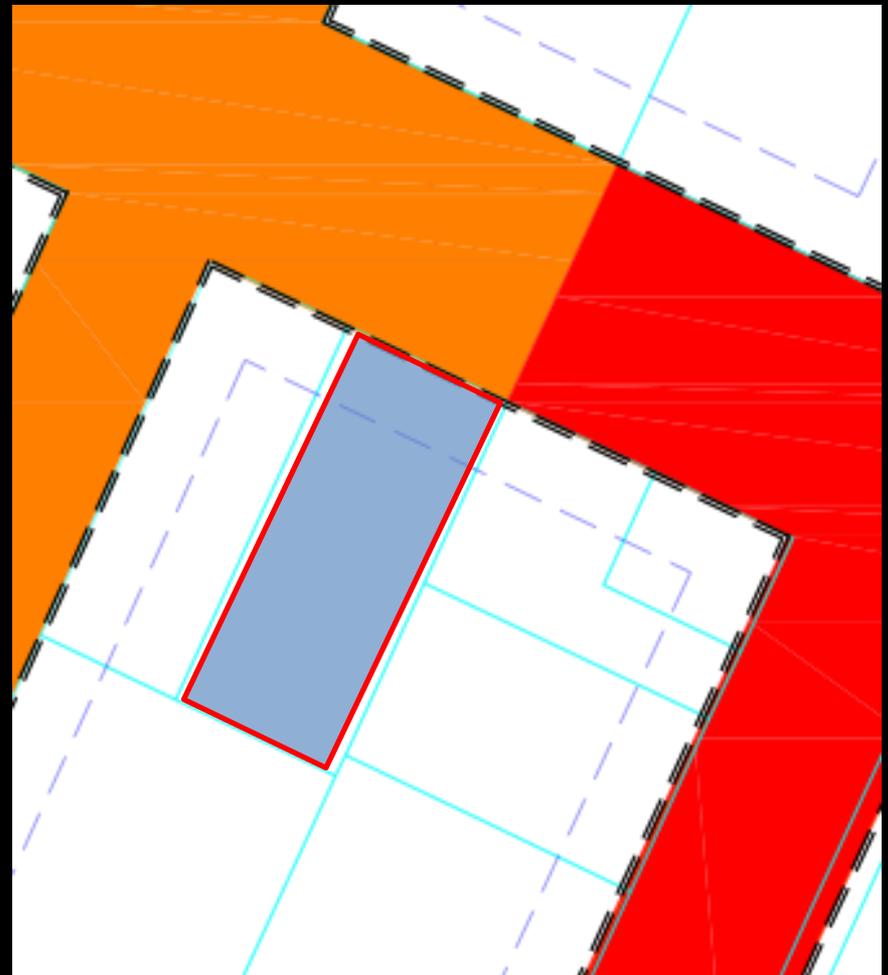
- Form District Boundary
- Required Building Line
- Parking Setback Line
- Parcel Line
- Townhouse - Small Apartment
- General Flex 3
- Storefront
- New Streets

DRAFT



Grand River Reg Plan:

- Sets district boundary
- Assigns Building Form (Part 4)
- Locates future streets (Part 5)



Okemos Reg Plan:

- Same as Grand River *plus*
- Locates RBL (Required Building Line)
- Locates Parking Setback Line



Parking Setback

RBL

55'
3815 gross
sq
FOOTPRINT

Part 4: Building Form Standards

	Required Open Space
	Townhouse - Small Apt.
	General Flex 3 (Number = Max Height)
	General Flex 5
	District Boundary
	Required Storefronts
	Riverfront Path
	New Street
	Refer to Downtown Okemos Reg. Plan

- Frontage Type (from Reg Plan): rules about **height, placement, building elements, uses**
 - Townhouse / Small Apartment
 - General Flex Avenue
 - General Flex Village
 - Storefront (Village only)

403. General Flex Village

ILLUSTRATIONS AND INTENT

Note: These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards on the following pages for the specific prescriptions and restrictions of the General Flex Building form standard.

This is the basic urban STREET-SPACE FRONTAGE. It fully defines the street edge and accommodates a range of uses, including retail, office, institutional, and residential. This frontage is in the most intense areas, generally close to the center of the District. It is anticipated that there will be significant pedestrian traffic along this frontage type.



Mixed-use building entrance



Residential building



Mixed-use buildings



Storefront streetscape



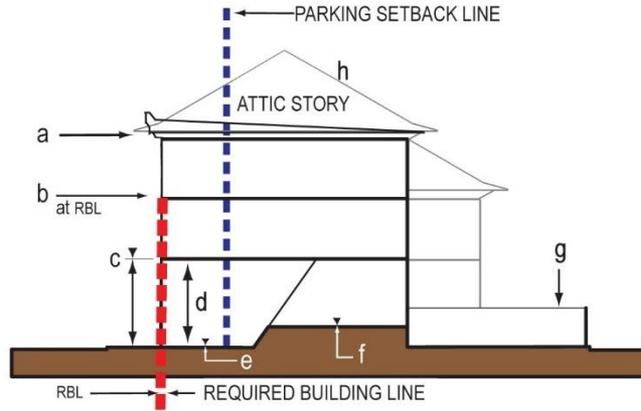
Residential building entrance



Storefront entrance



Mixed-use buildings with 'tower'

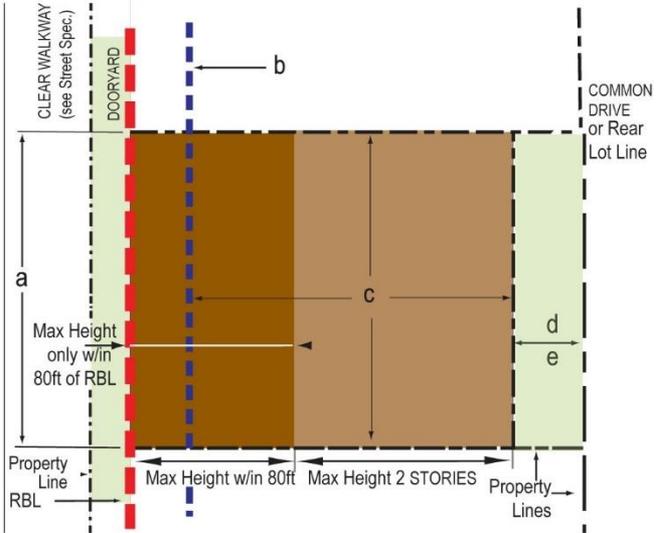


HEIGHT

		General Flex 3
a	Max STORY Height ¹	3
b	Min STORY Height	2
	Ultimate Building Height ¹	35'
c	2nd Floor Elevation ²	18' to 22'
d	Ground Floor Clear Height, COMMERCE ³	-4" to +18"
e	Ground Floor Elevation, COMMERCE ³	15' min
f	Ground Floor Elevation, Residential Units ⁴	3' to 8'
g	PRIVACY FENCE	7' Max
	STREET WALL	4' to 8'
h	Visible Roof Pitch ⁵	Min 4/12 Max 12/12, MANSARD configurations: lower pitch: Min 20/12 Max 45/12 upper pitch: Min 4/12 Max 8/12

Notes:

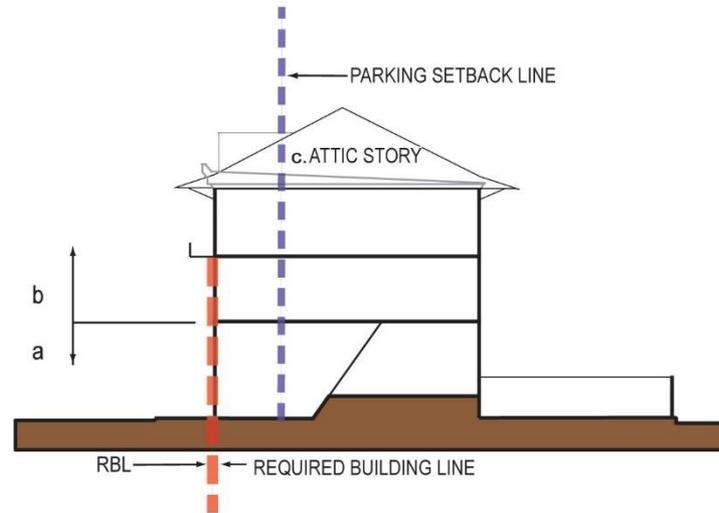
- Where a BONUS STORY is awarded by the Township, the STORY height will increase by 1 and the Ultimate Building Height by 10'.
- Measured from garage front sidewalk elevation to the 2nd story finished floor. This is to ensure adequate height



PLACEMENT

		General Flex 3
a	Frontage Build-To, Min	70%
b	PARKING SETBACK LINE ¹	30'
c	PRIVATE OPEN AREA, Location, Min Area ²	10%
	Side Setbacks	None
d	Rear Setback, COMMON DRIVE	8'
e	Rear Setback, no COMMON DRIVE	25'
	STREET WALL	Required on unbuilt RBL
	PRIVACY FENCE ³	Permitted, Side or Rear Lot Lines
	Footprint Max. ⁴	15,000 sf

- Notes:**
1. Reduced to 12ft where a minimum 5ft STREET WALL is built.
 2. At least 67% of the PRIVATE OPEN AREA is required to be at grade.
 3. Except where NEIGHBORHOOD MANNERS is in effect, see *Section 405.B*.
 4. For grocery stores, the maximum footprint is doubled.

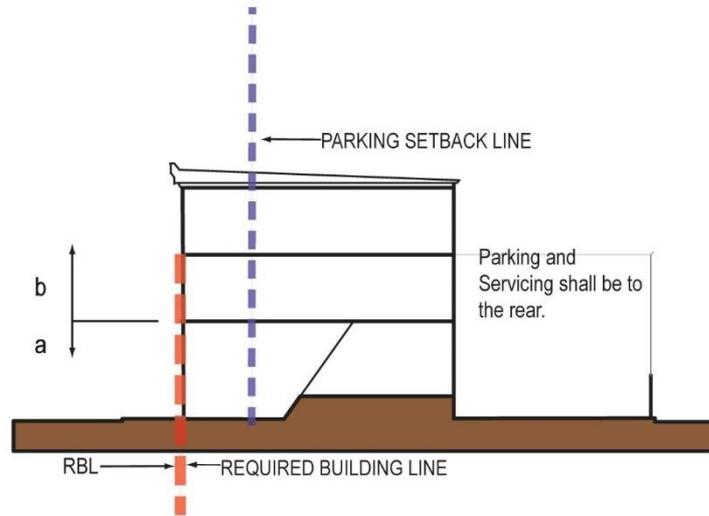


ELEMENTS

		General Flex 3
a	Ground STORY FENESTRATION	40 to 80%
b	Upper STORY FENESTRATION	25 to 80%
c	ATTIC STORY	One level permitted per 402.C.3
	FAÇADE: Max door to door ¹	75'

Notes:

1. On the FAÇADE of an individual building or FAÇADE COMPOSITION.



USES

		General Flex 3
a	Ground STORY ¹	COMMERCE, RESIDENTIAL ²
b	Upper STORIES ³	COMMERCE, RESIDENTIAL

Notes:

1. Support functions for RESIDENTIAL and hotel uses, such as lobbies, rental offices, and club rooms may be located at grade.
2. Street entrances shall be at grade, with transitions to meet the minimum finished floor elevation for RESIDENTIAL units within the building interior.
3. No COMMERCE (INCLUDING RETAIL) above RESIDENTIAL.

- General Provisions that apply to all (start p 16):
 - Height
 - Placement
 - Elements
 - On-site Parking and Access (curb cuts)
 - Frontage Designation Flexibility
 - Civic Buildings

- Special Requirements for General Flex and Storefront Frontages (start p 33):
 - Façade Composition
 - Neighborhood Manners
 - Bonus Height
 - Private Open Area

your questions:

Why is residential ground finished floor raised?



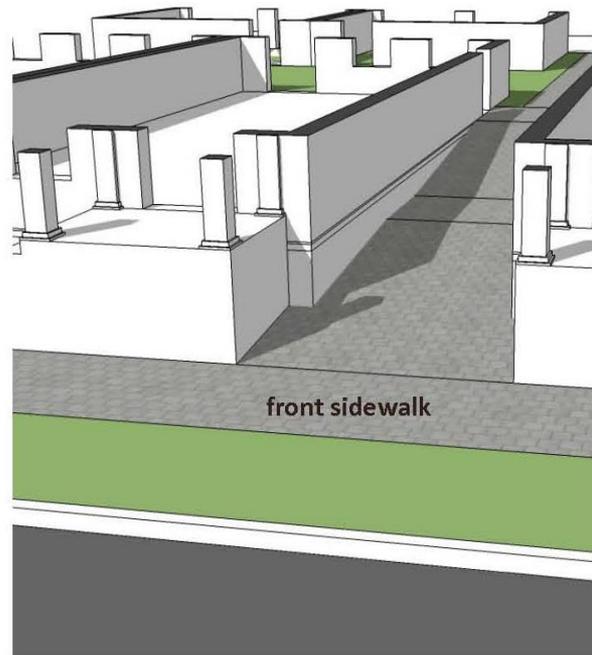
Without raised finished floor: reduced privacy; ground-floor units less desirable; reduced unit values; curtains stay closed; fewer eyes on the street

your questions:

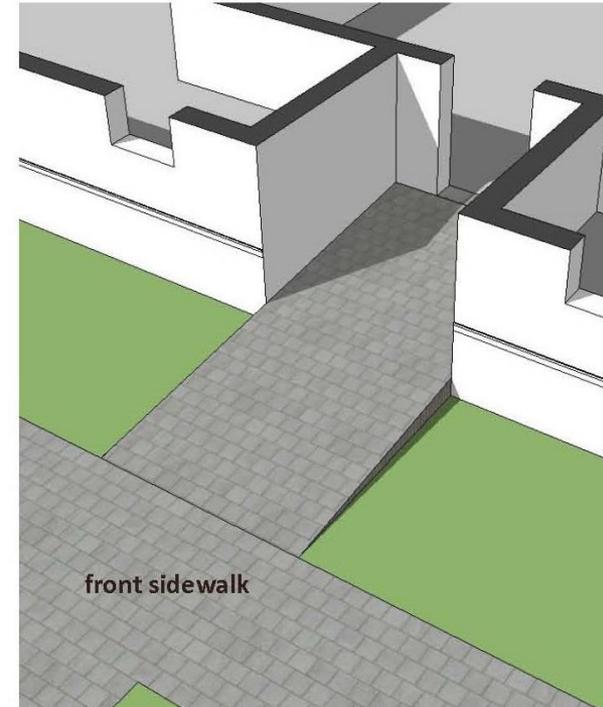
How do you make the building accessible?



Elevated first finished floor accessed by interior ramp



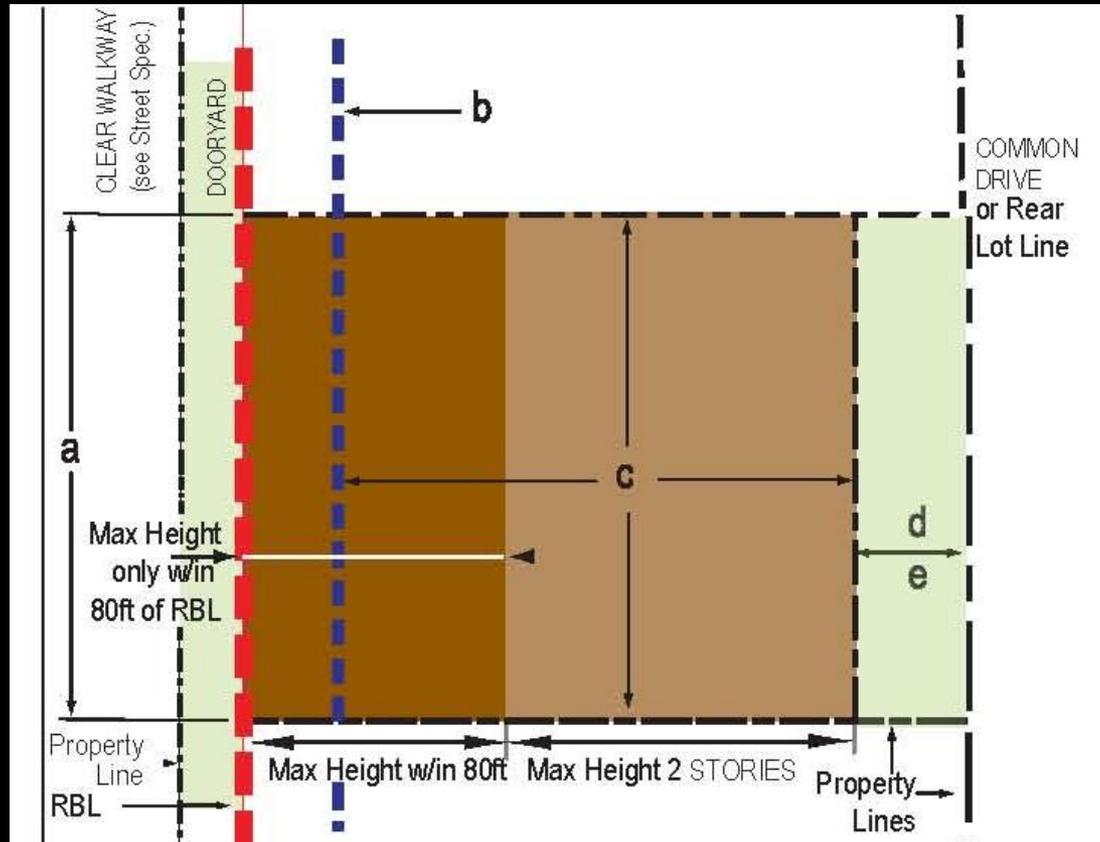
A subtle and well-integrated ramp set behind the building's front facade plane



Ramp from the front sidewalk to the front door

your questions:

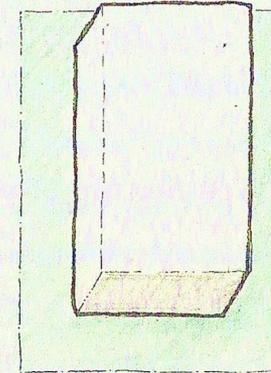
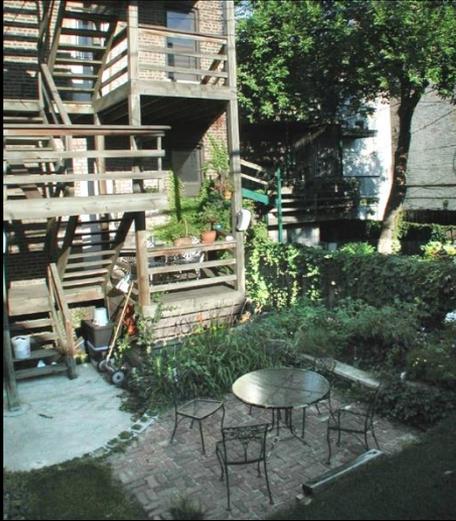
What is private open space, how does it work?



In General Flex: 10% of area "C", Townhouse: 15% of area "C"
67% in no more than 2 areas; Gen Flex 5 may be above grade

your questions:

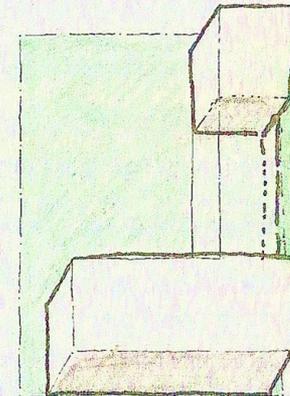
What is private open space, how does it work?



**CONVENTIONAL
DEVELOPER
SITING**

3,000 sq. ft Bldg.
7,000 sq. ft Lot

Front, Side and
Rear Setbacks.



**"URBAN"
SITING**

3,000 sq. ft Bldg.
7,000 sq. ft Lot

Front and Side
Build-To's

your questions:

Where do Neighborhood Manners apply?

General Flex

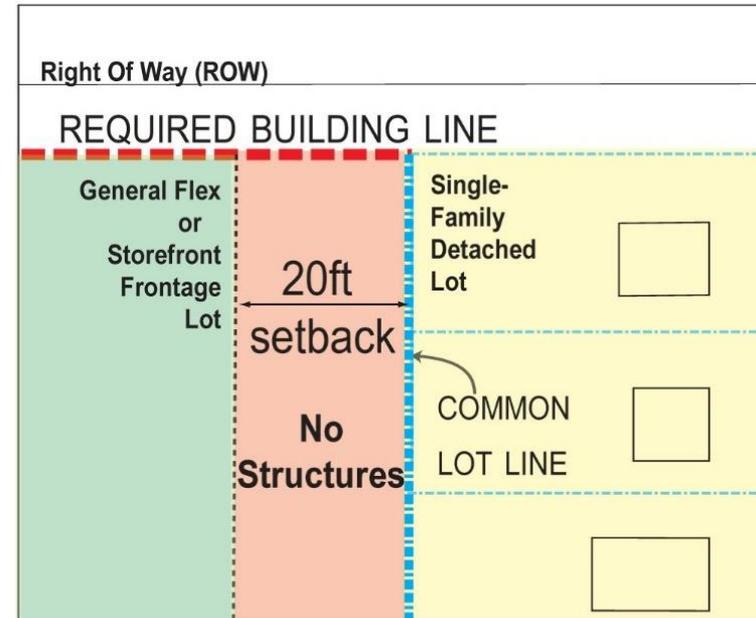
B. Neighborhood Manners

Additional rules to ensure a comfortable and complementary relationship with adjacent single family detached dwellings.

For the General Flex and Storefront BUILDING FORM STANDARD frontages sharing a COMMON LOT LINE with a single-family residential zoning district outside the Form District the following rules apply.

1. A GARDEN WALL, 4 to 8 feet in height, shall be constructed within one (1) foot of the COMMON LOT LINE.
2. Trees from the *Street Tree List* shall be planted, on maximum 30-foot centers, between 5 and 10 feet from this wall.
3. Neighborhood Manners Setbacks

Storefront



405.B.3.a Neighborhood Manners 20ft Setback

3. Neighborhood Manners Setbacks

a. There shall be a 20 foot setback from the COMMON LOT LINE. Trees and surface parking are permitted within this setback, but there shall be no structures within this area. (See *diagram 4.05.B.3.a*) If the setback area is used as a COMMON DRIVE the trees required by this section may be planted within this area.

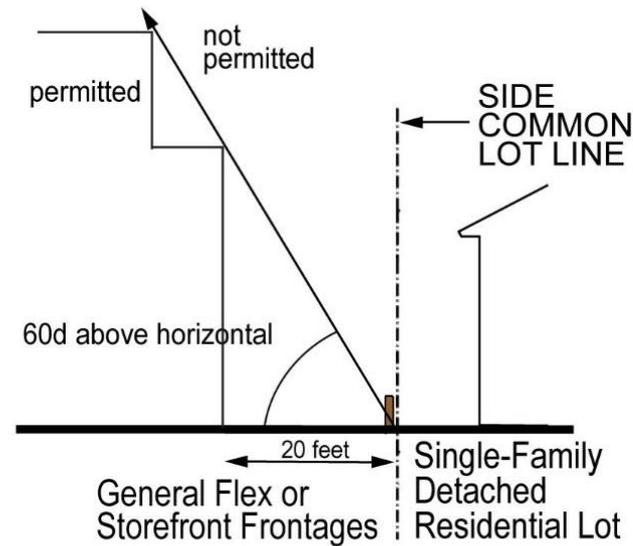
b. There shall be an additional setback plane:

(i) For COMMON LOT LINES that are within 80 feet of and perpendicular to a REQUIRED BUILDING LINE, extending at an angle of 60 degrees above horizontal, beyond which no building or structure is permitted.

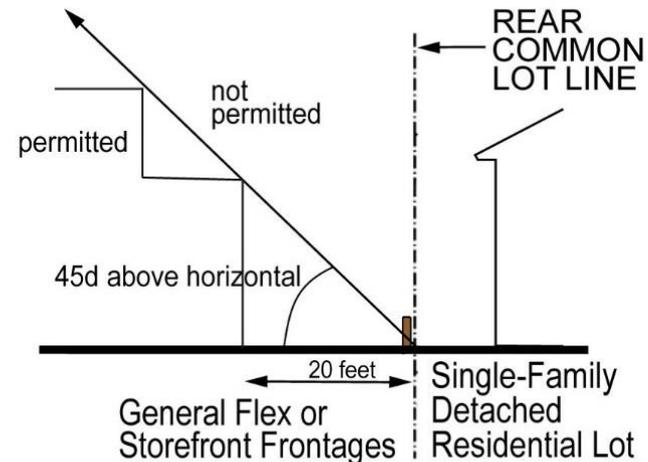
(ii) For COMMON LOT LINES that are more than 80 feet from a REQUIRED BUILDING LINE, extending at an angle of 45 degrees above horizontal, beyond which no building or structure is permitted.

4. The Neighborhood Manners Setback Area shall be adjusted with any frontage change per *402.E Frontage Designation Flexibility*.

405.B.3.a Neighborhood Manners 20ft Setback



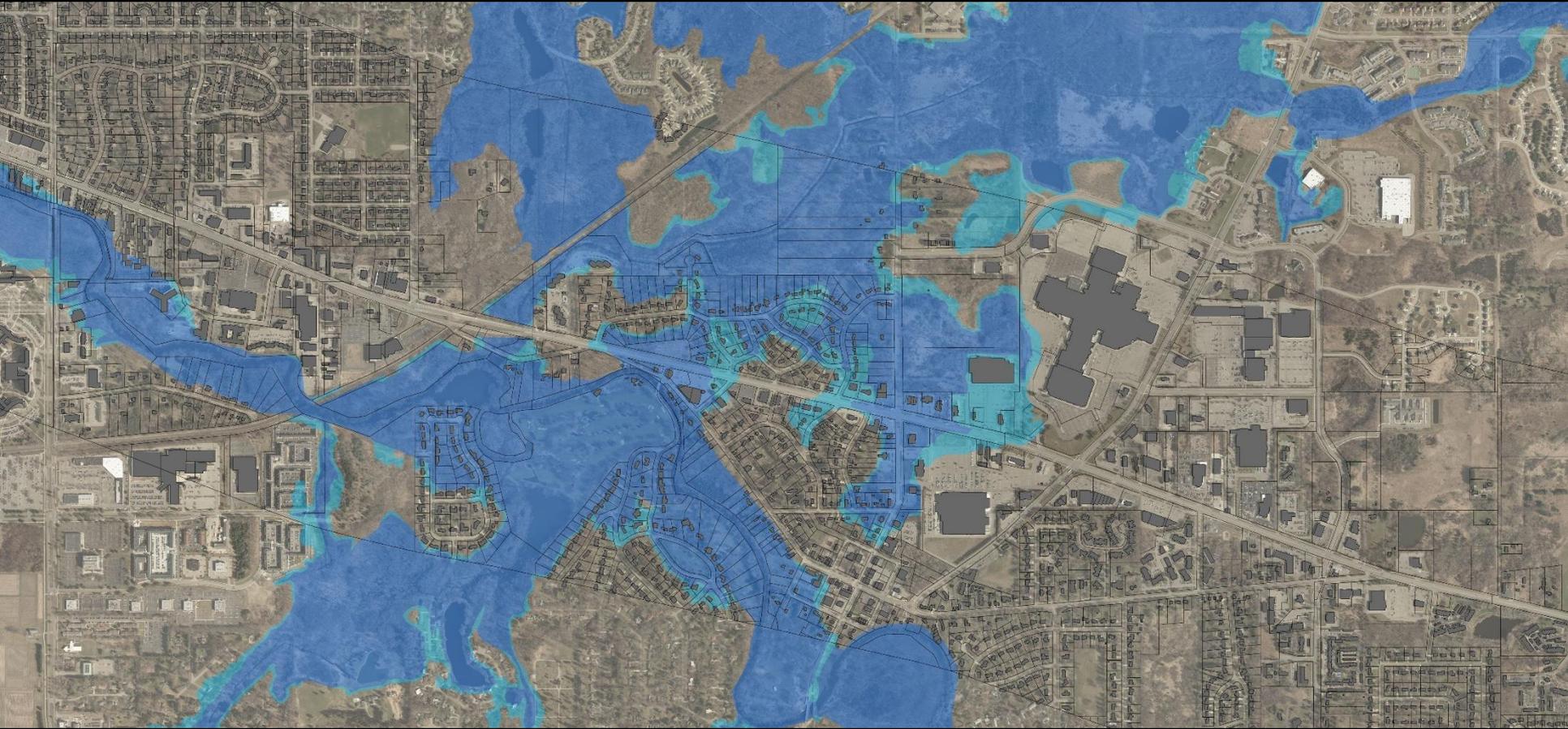
405.B.3.b.i Neighborhood Manners Setback Plane.



405.B.3.b.ii Neighborhood Manners Setback Plane.

your questions:

How is the RBL impacted by the presence of the floodplain?



Part 5: Public Realm Standards

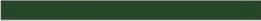
- Street Type Specifications
- Bus Stop Connectivity & Location Guidelines
- Mid-Block Pedestrian Crossing Guidelines
- Streetscape Standards
- Squares and Civic Greens

Public Realm Standards - Applicability

- Intent: Promote a more walkable street system in collaboration with applicants, Ingham County Roads Department, and MDOT
- Township CIA or DDA Right-of-Way and public space projects
- Streetscapes for new developments
- Road reconstruction projects
- Any new street / side access lane or redesigned street



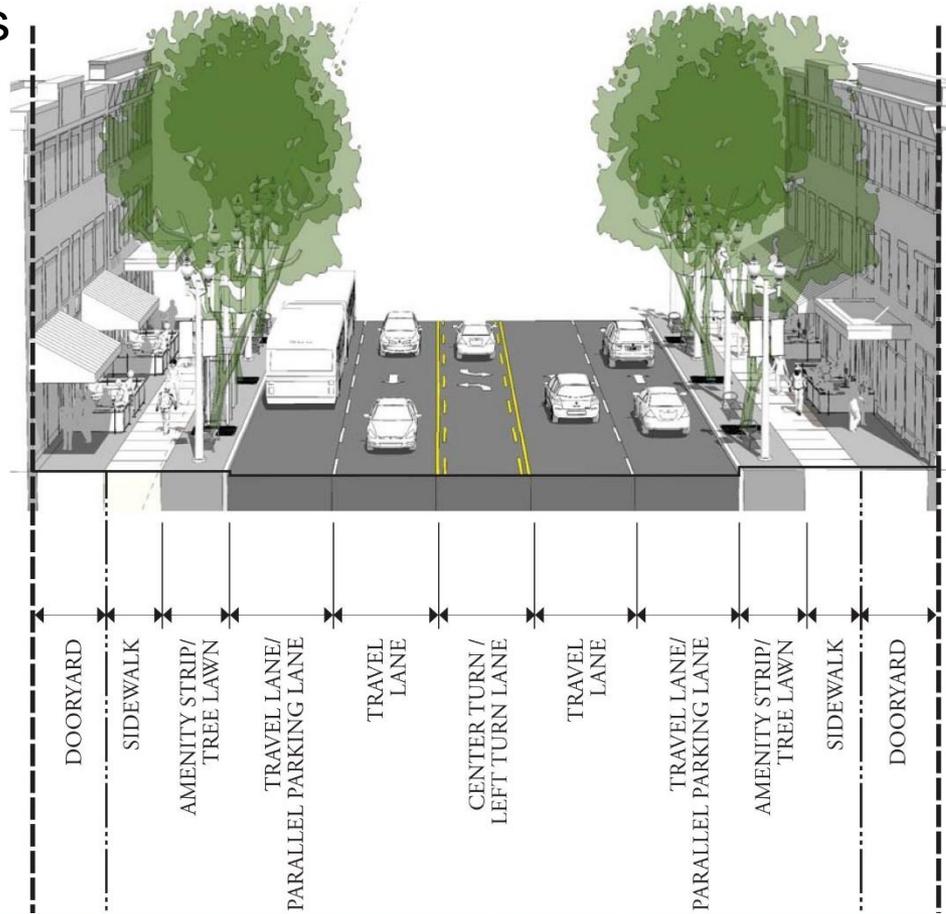
LEGEND

-  NS 80-46
-  NS 82-54
-  NS 78-44
-  NS 70-36
-  NS 70-36 (Proposed Street)
-  MA 252-54
-  MA 140-54

NOTE:

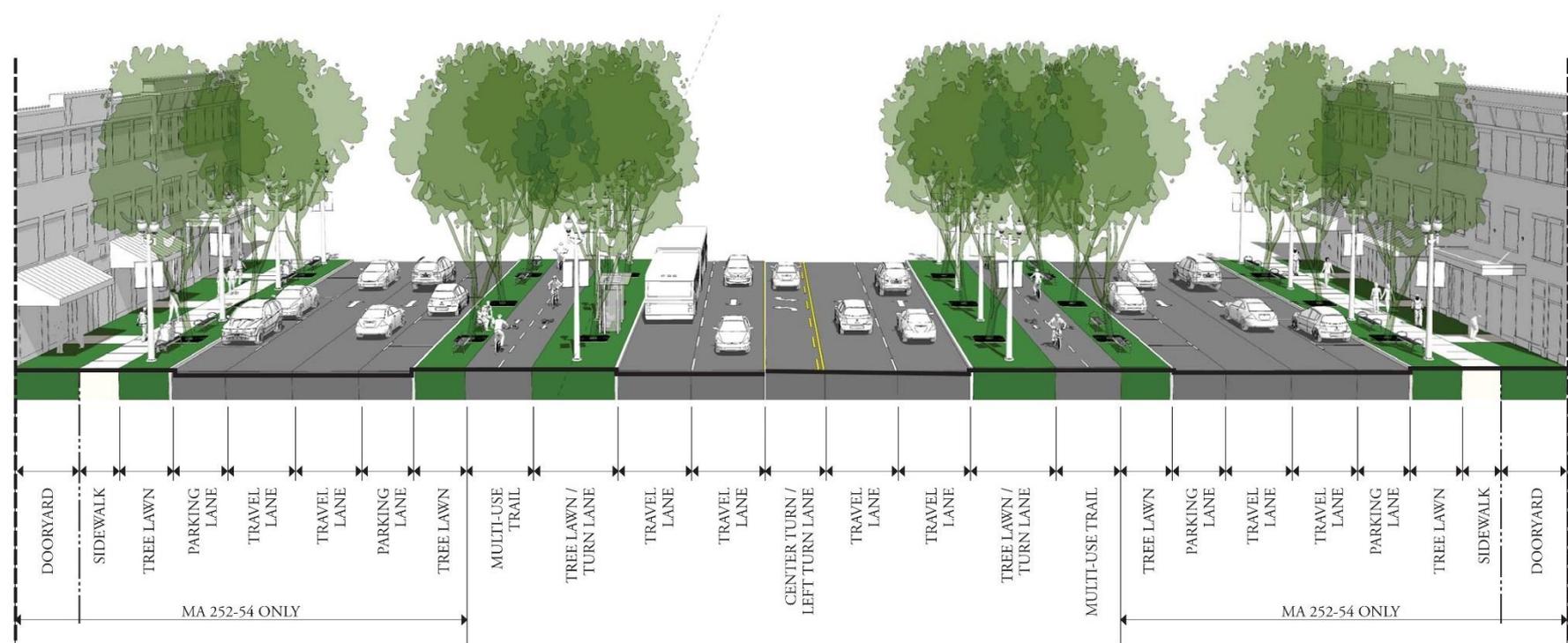
The numbers refer to dimensions within the STREET-SPACE. The first number is the literal STREET-SPACE (the distance between FAÇADES) and the second is the distance curb-face to curb-face (see cross-section specifications at the end of this section).

Neighborhood Streets



Street Type	Min. Right-of-Way	Travel Lane Width	Center Turn Lane Width	Parallel Parking Lane Widths	Amenity Strip Width	Tree Lawn Width	Sidewalk Width	Multi-Use Trail Width	Dooryard	Total Minimum Street Space
NS 80-46	78'	11'	10'	7' (optional) ¹	8' Min.	-	7'	-	1'	80'
NS 82-54	80'	11'	10'	-	6' Min.	-	6'	-	1'	82'
NS 78-44	76'	11'	-	7' (optional) ¹	6' Min.	-	6'	-	1'	78'
NS 70-36	60'	10'	-	8'	-	6' min.	6'	-	5'	70'
MA 252-54	232'	11'	10'	8'	8' Min.	-	6'	10'	10'	252'
MA 140-54	110'	11'	10'	-	12' Min.	-	-	10'	15'	140'

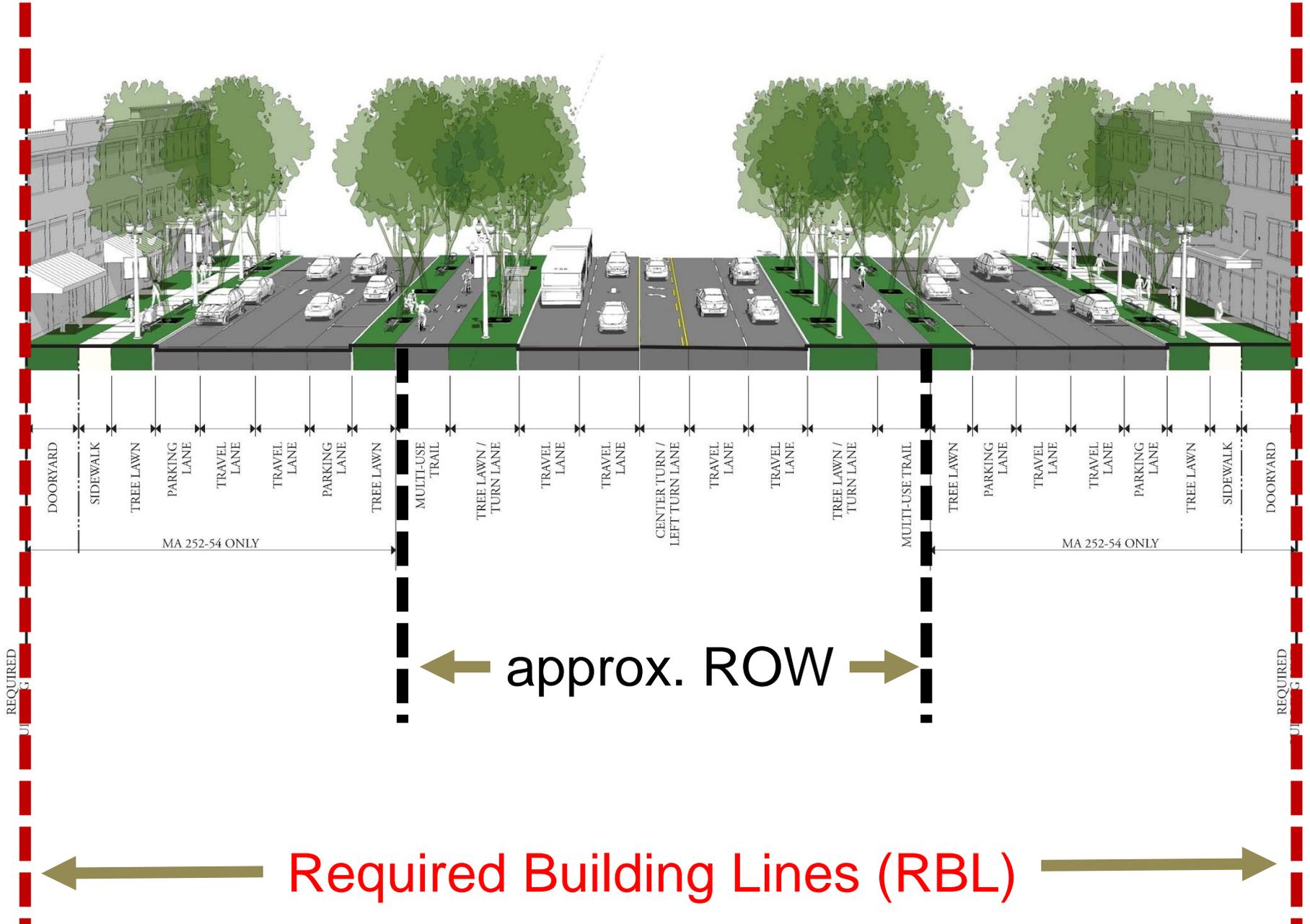
Major Arterial Street (Grand River Ave)



REQUIRED BUILDING LINE

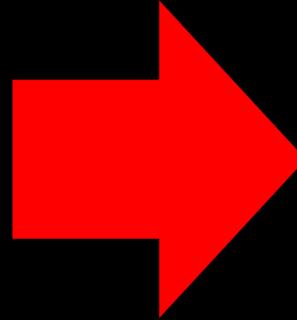
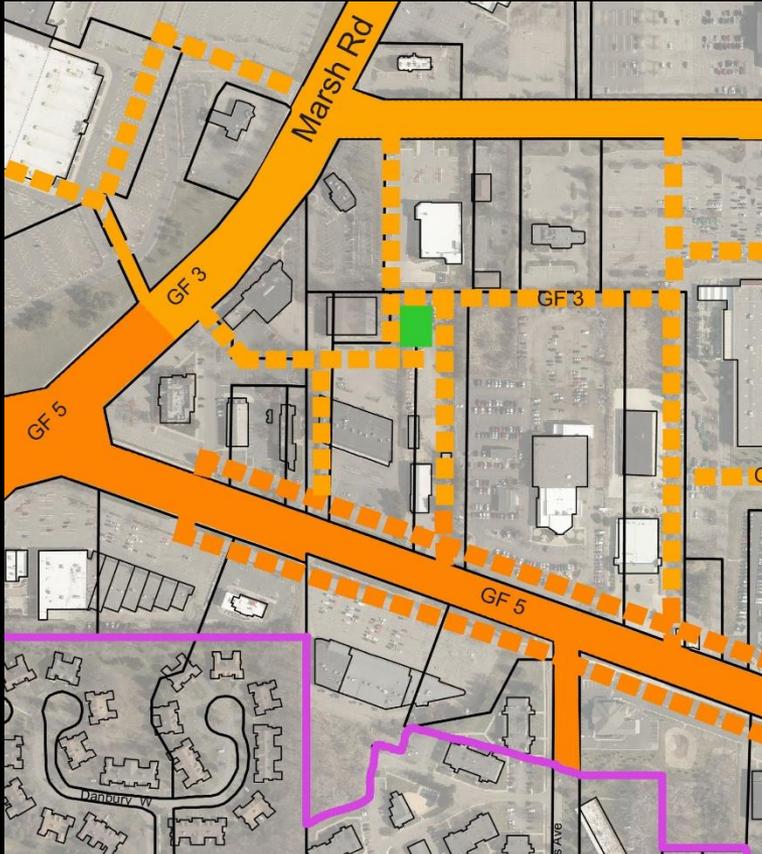
REQUIRED BUILDING LINE

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NS 82-54	80'	11'	10'	-	6' Min.	-	6'	-	1'	82'
NS 78-44	76'	11'	-	7' (optional) ¹	6' Min.	-	6'	-	1'	78'
NS 70-36	60'	10'	-	8'	-	6' min.	6'	-	5'	70'
MA 252-54	232'	11'	10'	8'	8' Min.	-	6'	10'	10'	252'
MA 140-54	110'	11'	10'	-	12' Min.	-	-	10'	15'	140'



your questions:

How do Regulating Plan and Street Atlas work?



Section 86-625
Oleaves Village and Grand River Avenue Town District

403. General Flex Village

ILLUSTRATIONS AND INTENT

Note: These photos and statements are provided as illustrations of intent and are advisory only. They do not have the power of law. Refer to the standards on the following pages for the specific prescriptions and restrictions of the General Flex Building form standard.

This is the basic urban STREET-SPACE FRONTAGE. It fully defines the street edge and accommodates a range of uses, including retail, office, institutional, and residential. This frontage is in the most intense areas, generally close to the center of the District. It is anticipated that there will be significant pedestrian traffic along this frontage type.

Residential building
Mixed-use buildings
Streetfront streetscape
Residential building entrance
Storefront entrance
Mixed-use buildings with tower

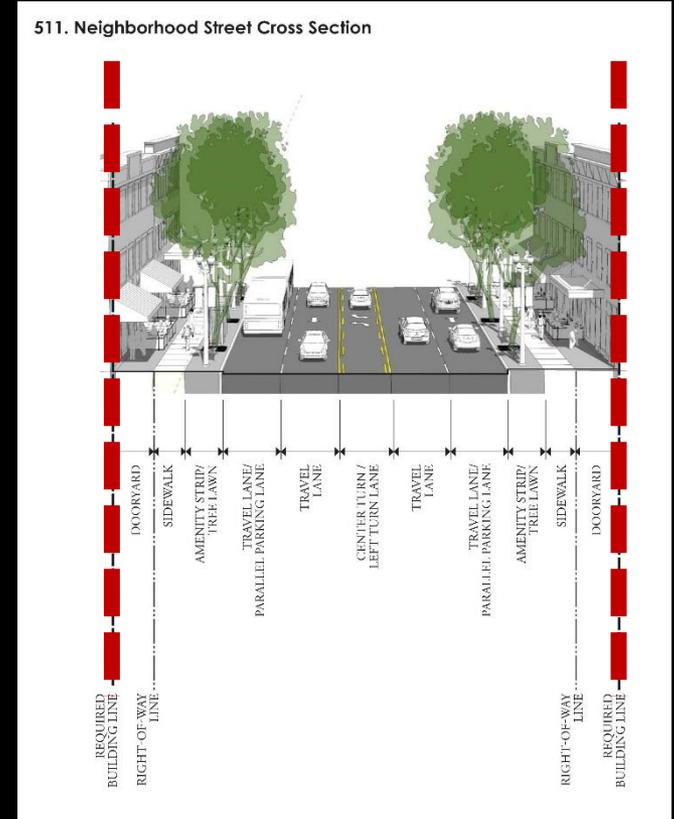
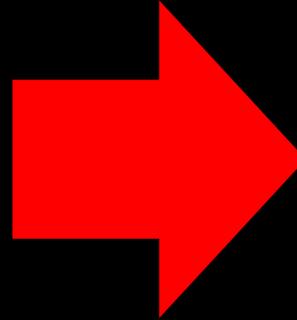
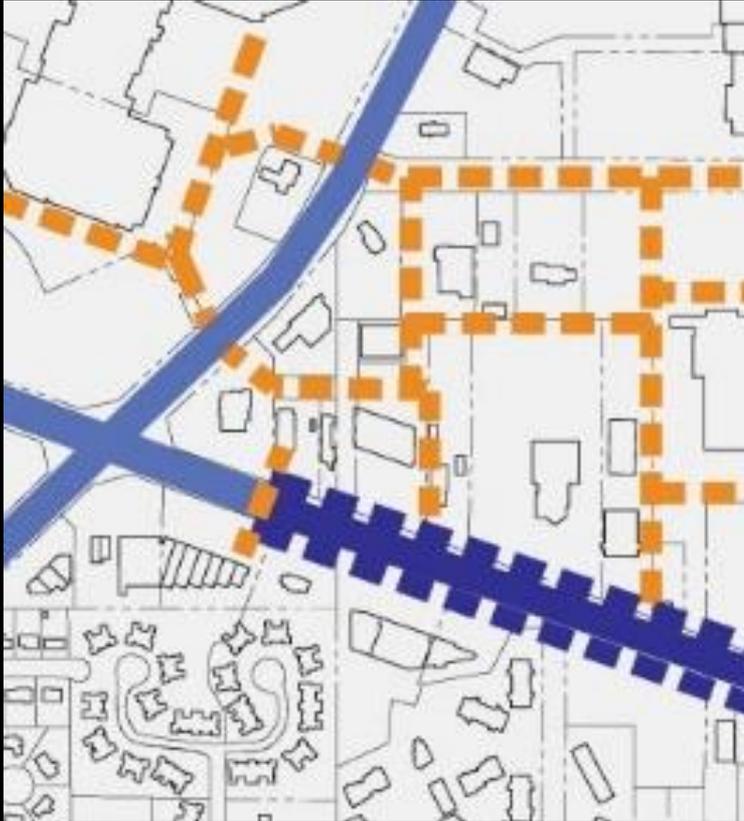
February 2018
REVIEW DRAFT 2

Shaping the Avenue
Mendota Township 27

Regulating Plan assigns **Frontage Type** and location of **new streets**

your questions:

How do Regulating Plan and Street Atlas work?



Street Atlas assigns **Street Type** and location of **RBL**

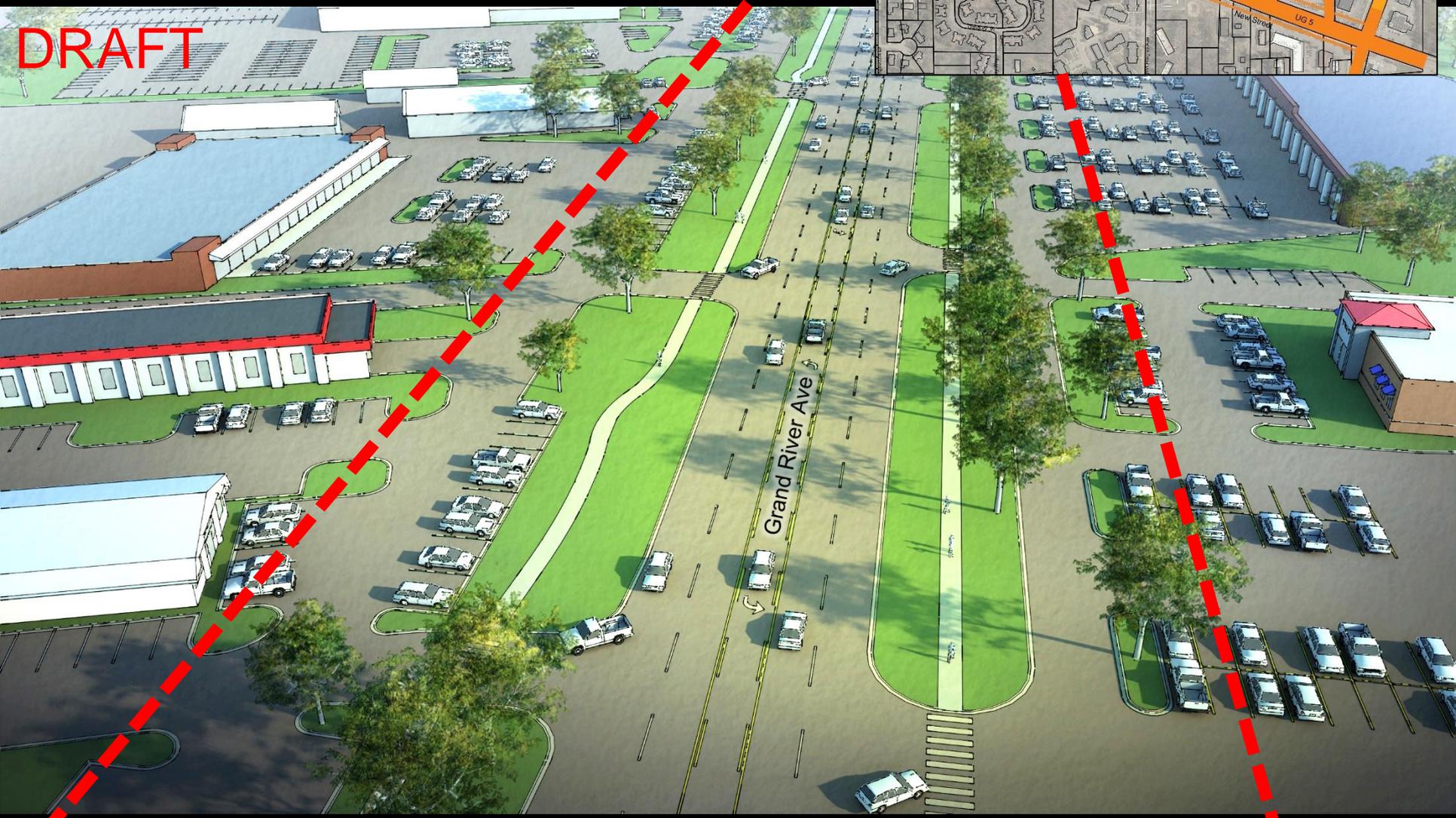
your questions:

What if Grand River Ave or other street designs are not implemented? The Township has no control over the roads.

Will the code work with the existing street designs?

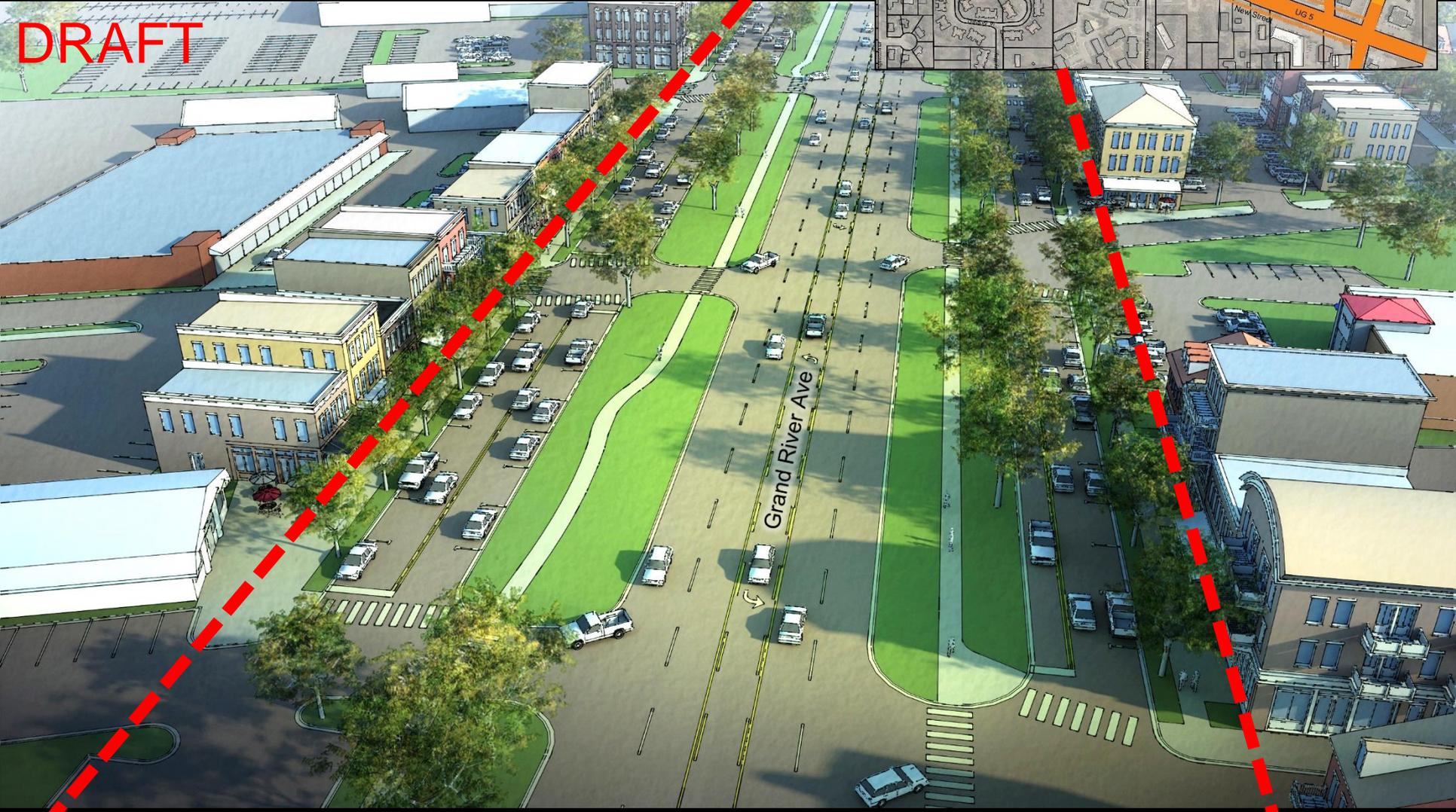
How do we account for piecemeal look of development if code is optional on Grand River Ave?

DRAFT

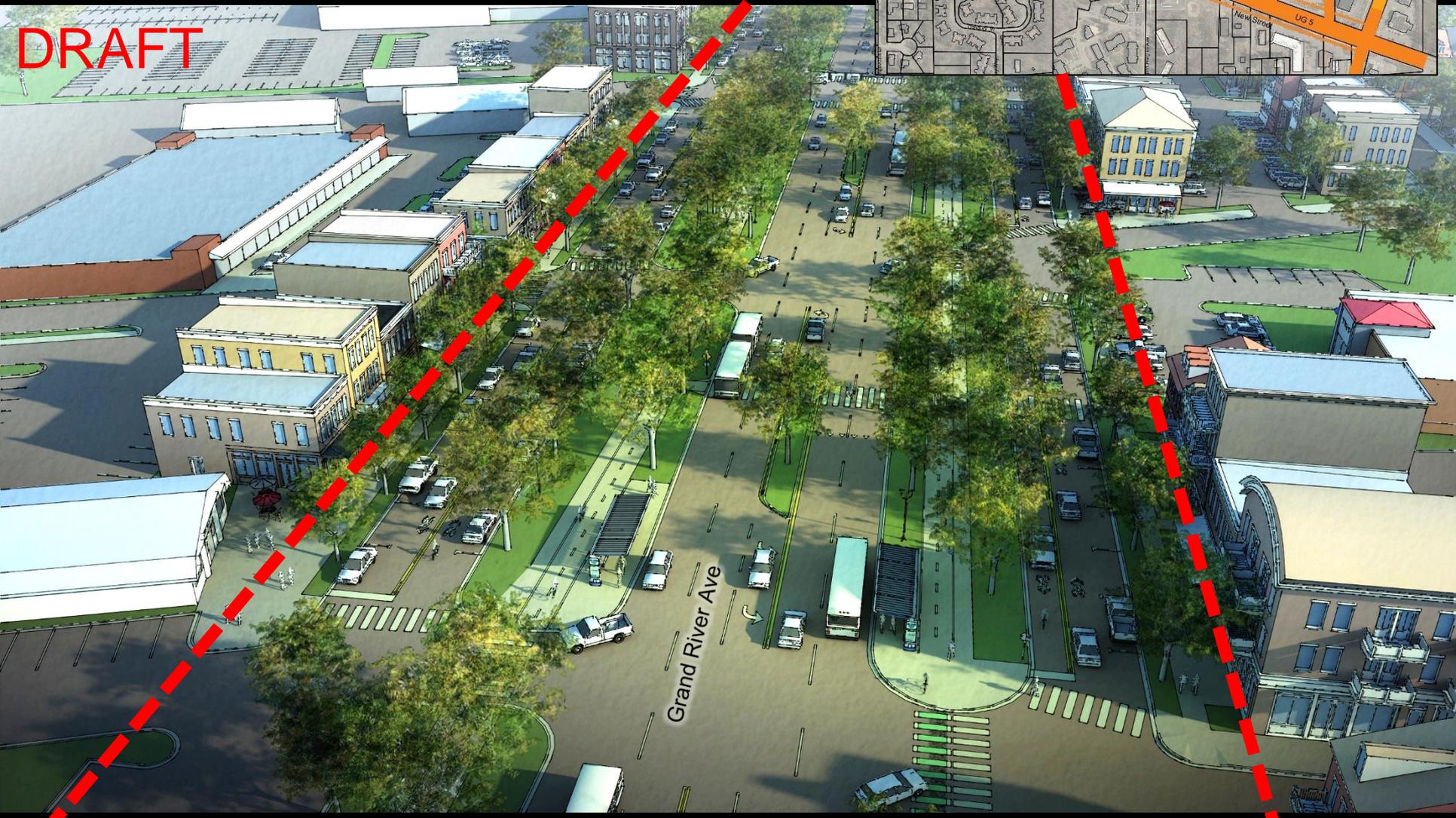


Existing Conditions along Grand River Avenue

DRAFT



Potential for change-over-time along Grand River Avenue



DRAFT



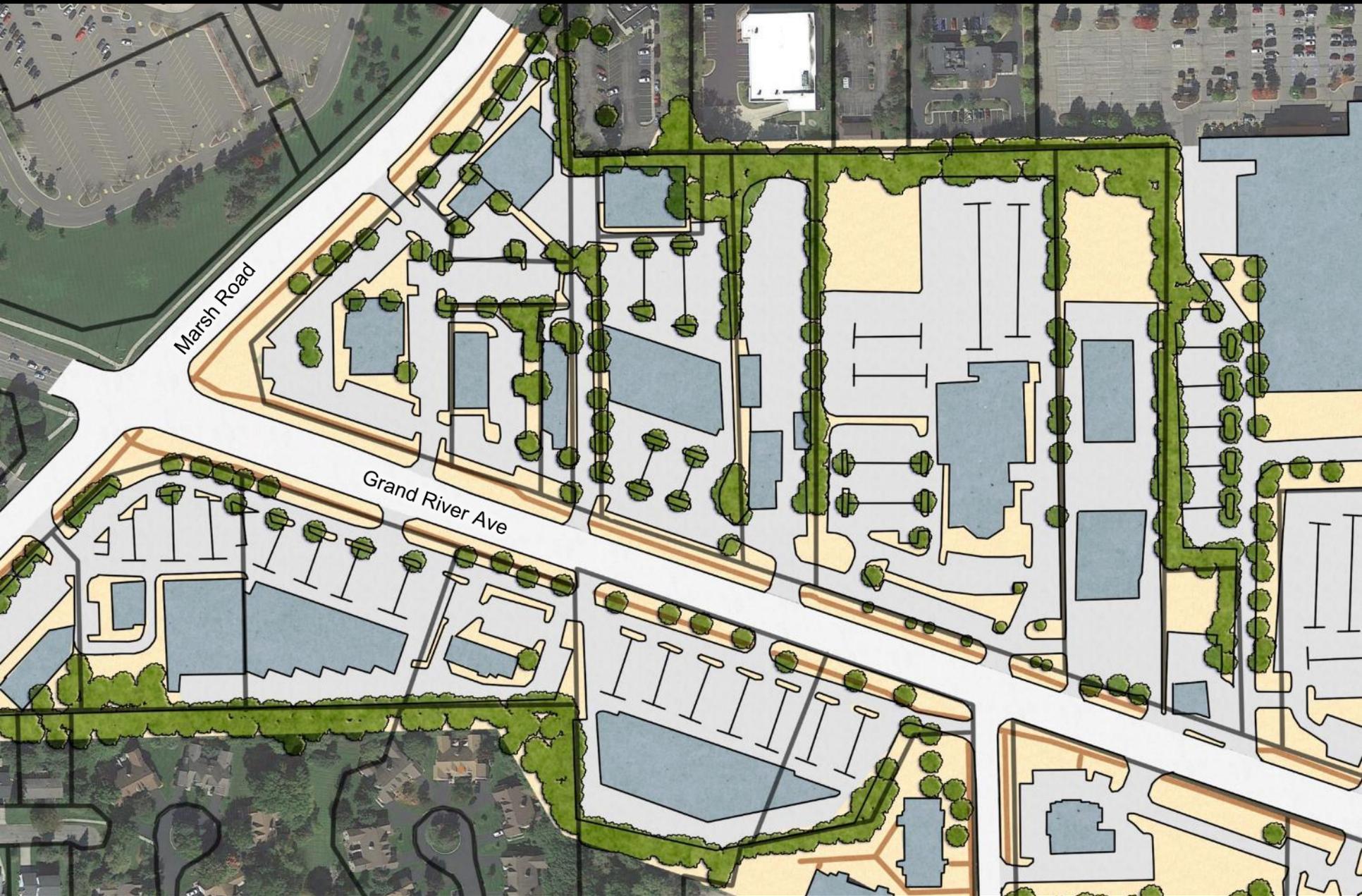
Potential for change-over-time along Grand River Avenue



Marsh Road

Grand River Ave

Existing Conditions along Grand River Avenue

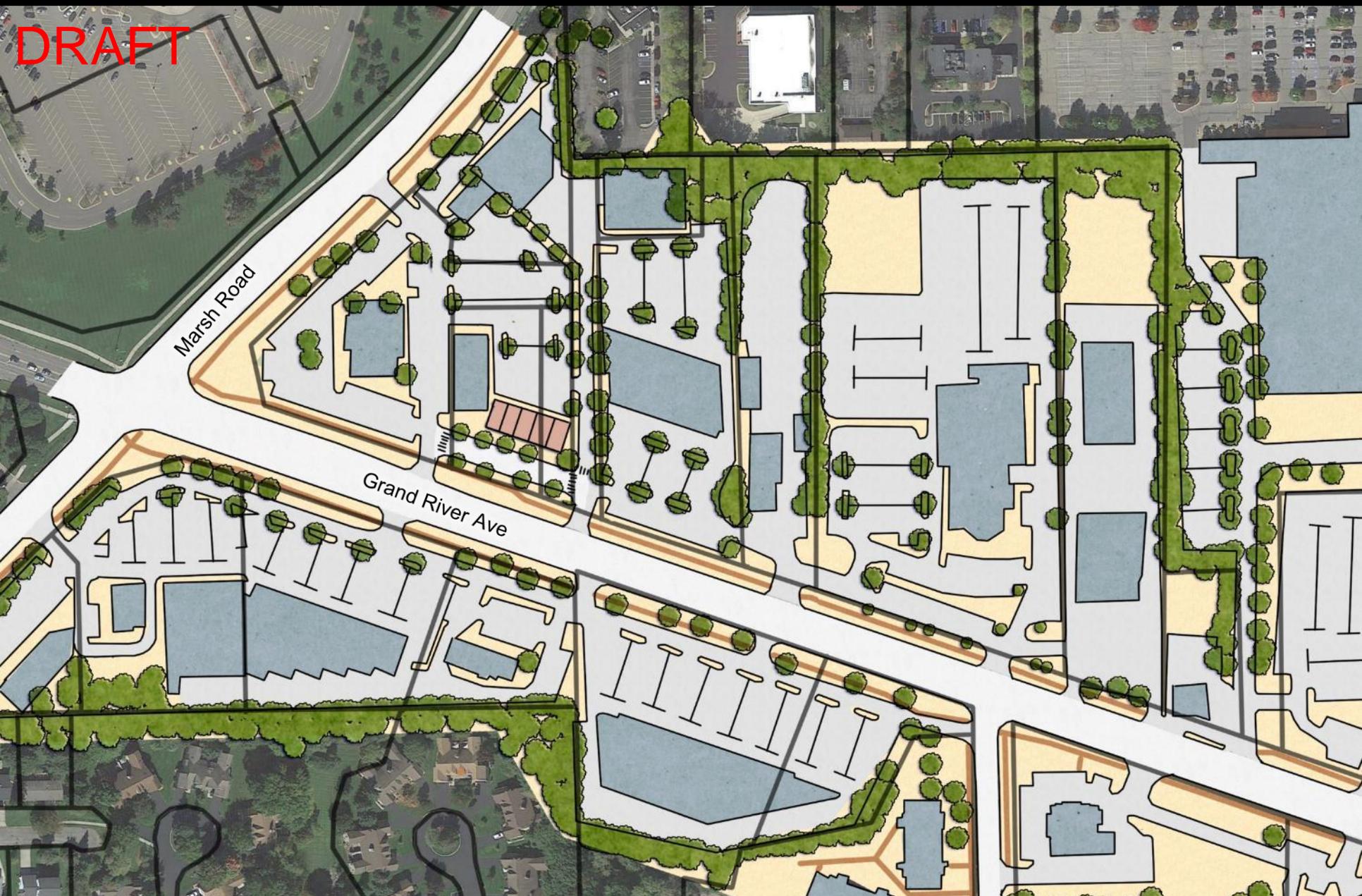


Marsh Road

Grand River Ave

Existing Conditions along Grand River Avenue

DRAFT



Marsh Road

Grand River Ave

Potential for change-over-time along Grand River Avenue

DRAFT



Marsh Road

Grand River Ave

Potential for change-over-time along Grand River Avenue

DRAFT



Marsh Road

Grand River Ave

Potential for change-over-time along Grand River Avenue

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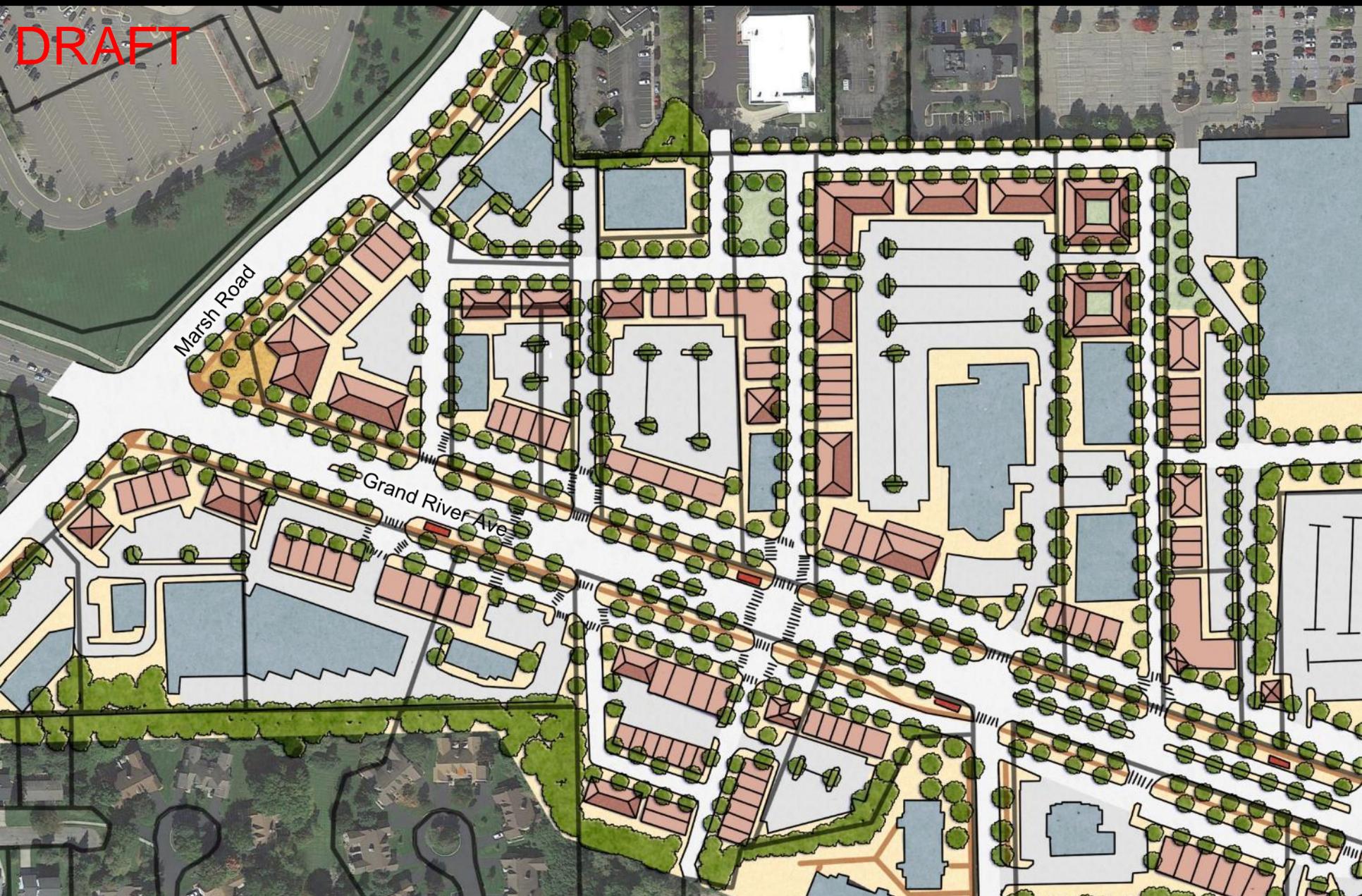


Marsh Road

Grand River Ave

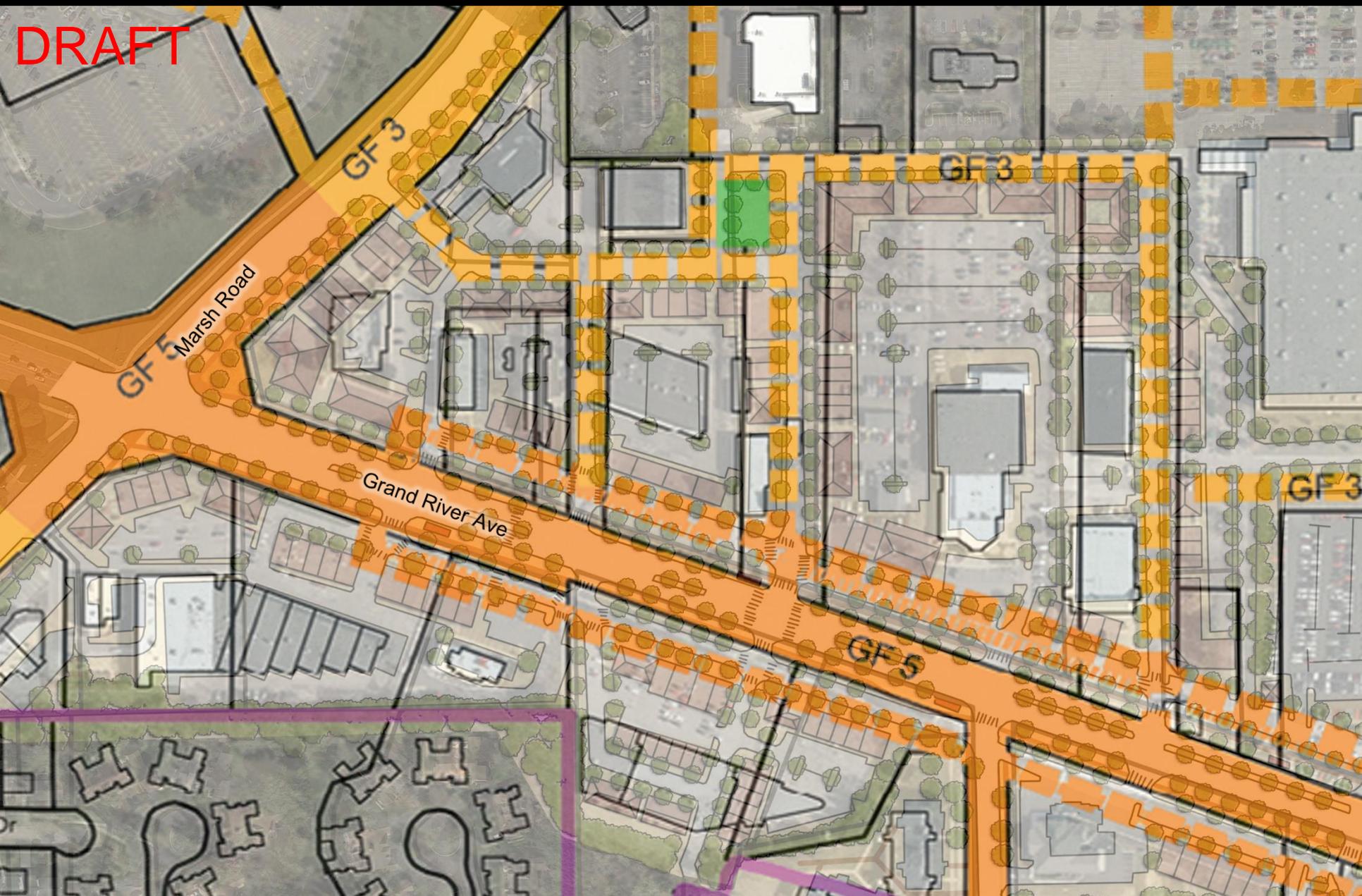
Potential for change-over-time along Grand River Avenue

DRAFT



Potential for change-over-time along Grand River Avenue

DRAFT



Potential for change-over-time along Grand River Avenue

your questions:

When/how are street improvements done?

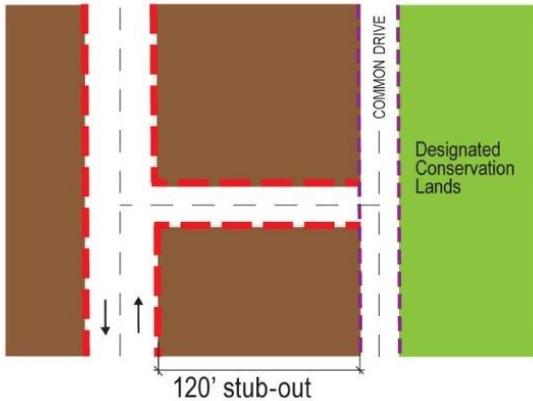


Figure 303.1.b.iii

2. New Streets

In addition to the street connectivity standards above, the following standards apply to those streets constructed after the adoption of this district.

- a. New streets designated on the REGULATING PLAN may or may not be immediately constructed.
 - (i) Any new streets that create building frontage on the parcel being developed shall be constructed at the time of development.
 - (ii) Public streets shall be placed into the system such that, when reasonable in the context of street maintenance or capital improvements plan, they can be constructed.
 - (iii) Private streets must be similarly scheduled for construction by the developer.
- b. If constructed within 50 feet of the designated center line location, these streets may be administratively approved, provided the resulting configuration meets these street configuration standards and that any other properties with frontage are not adversely effected.

your questions:

When/how are street improvements done?

Streetscape Elements Redevelopment Timing Matrix	Install new street trees (per 506.B)	Install street furniture (per 506.C)	Install/upgrade streetlights (per 506.C)	Install/upgrade sidewalks/shared pathways (to meet standard 506.C)	Construct pedestrian connections btwn sidewalks & buildings	Reconstruct Grand River frontage service drives (to meet standards in 509.B)	Install bike parking (per 506.D)	Close driveways that do not meet access standards (M-43 Access Mgmt. Plan)	Install / upgrade bus stops and transit amenities
Require for new site plans. Project funded by developer.	X			X	X	X	X	X	X
Require a financial guarantee or payment in lieu of changes. Funds go into a pool for future implementation when there is a larger project area.									
Project funded by the CIA or DDA. Funding assistance could be provided for closing driveways and redesign of frontage roads and access changes. If property owners participate, funding will be covered by the project. If not, property owners pay for it themselves later.	X	X	X	X			X	X	
Wait to include with ICRD/ MDOT project to reconstruct the road curb-to-curb. Since they are not likely to budget for all the desired improvements, this is the least preferred option.								X	
Funded by CATA.									X

Part 6: Architectural Standards

- Building Walls
- Roofs & Parapets
- Street Walls
- Windows & Doors
- Signage
- Lighting & Mechanical

Section 66-445
Olemiss Village and Grand River Avenue Form Districts

606. Windows and Doors

A. Purpose and Intent

The placement, type, and size of windows and doors on the FACADE largely establishes the scale and character of the STREET-SPACE. For retail buildings, windows allow interplay between the SHOPFRONT interiors and the STREET-SPACE. Commercial uses (especially restaurants and retail establishments) benefit from exposure to the passers-by and the STREET-SPACE benefits from the visual activity. For residences, windows foster the "eyes on the street" surveillance which provides for the security and safety for the area.

Windows should be divided by multiple panes of glass to provide a pedestrian scale.

B. Applicability

The standards in this section shall apply to any window or door that is CLEARLY VISIBLE FROM THE STREET-SPACE.

C. Illustrations

Photographs are provided as illustrations of intent. The illustrations and statement on this page are advisory only. Refer to the district standards on the following pages for the specific requirements of this section. The text and diagrams on the following pages have the power of law. Where photographs or statements on this page may be inconsistent with the text, the text shall prevail.



Door with transom and sidelight windows



Multi-paned storefront windows and glass doors



Storefront window



Grouped windows



Storefront Frontage



Grouped windows

Part 7: Parking Standards

- Minimum Requirements (p 71)
- Shared Parking (p 71)
- Location: on-site or within 800'; must be on same side of Grand River or within 1/8 mile of crosswalk (p 72)

Part 7: Parking Standards

2 Note to Staff: the consultant team recommends that a Parking Management Strategy be developed for the Okemos Village district, and potentially Grand River form District, enabling more progressive parking standards for a true pedestrian- and transit-oriented environment. Parking ratios could be reduced accordingly, such as the following:

Commercial, general retail, restaurants, taverns & bars, nightclubs, distilleries & brew pubs:

Option A:

2.0 spaces per 1,000 sq. ft. (minimum) and 3.0 spaces (maximum) for buildings with a gross floor area less than 25,000 sq. ft.

3.0 spaces per 1,000 sq. ft. (minimum) or 4.0 spaces (maximum) for buildings with a gross floor area over 25,000 sq. ft.

Option B:

No minimum and 3.0 spaces maximum for buildings with a gross floor area less than 15,000 sq. ft.

2.0 spaces per 1,000 sq. ft. (minimum) or 3.0 spaces (maximum) for buildings with a gross floor area over 15,000 sq. ft.*

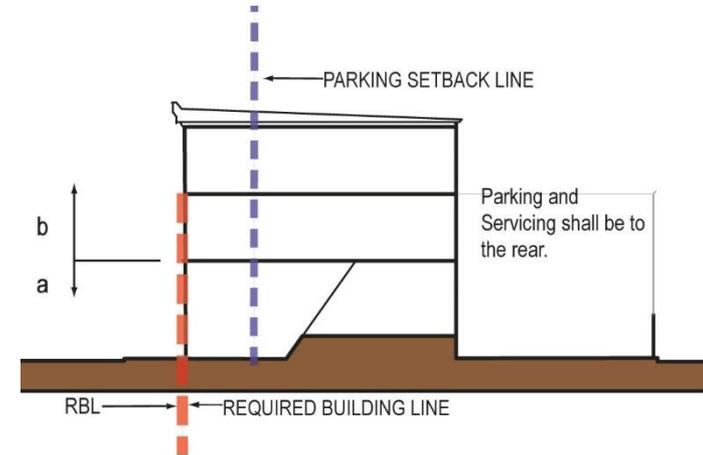
SHAPING THE AVENUE

UNIQUE PLACES AND COMMUNITY SPACES

Part 8: Building Uses

USE CATEGORY		BUILDING FORM STANDARD						
		General Flex		Storefront		Townhouse/ Small Apt		
		Ground Story	Upper Story	Ground Story	Upper Story	Ground Story	Upper Story	
RESIDENTIAL	Household Living	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	Sec. 804.B.1-2
	Group Living		<input type="checkbox"/>		<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Section 86-654 of the Township's <i>Code of Ordinances</i>
COMMERCE	Office	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>			Sec. 804.D.
	Overnight Lodging	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	Sec. 804.E.1-2
	Recreation/Entertainment	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>			
	Vehicle Sales	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>		<input checked="" type="checkbox"/>			Sec. 804.F.2
	Day Care	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>		<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	<input checked="" type="checkbox"/>	children: Section 86-403 (d)(1) adults: Section 86-403 (d)(2) of Township's <i>Code of Ordinances</i>
	Retail Sales & Service	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>			Sec. 804.F.2, 5, 6
	Restaurant/Bar	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>			Sec.804.F.1-4
	Vehicle service/ Gas Station	<input type="checkbox"/>						Sec. 804.G.
CIVIC	See Part 9. Definitions	<input checked="" type="checkbox"/>		<input checked="" type="checkbox"/>		<input checked="" type="checkbox"/>		Sec. 804.C.

Key: = Permitted = Additional Regulations Apply Blank Cell = Not Permitted



your questions:

In other communities, is governing body unwilling to give up authority on projects and delegate to staff?

your questions:

It would be helpful to take one or more hypothetical projects through the entire process under the draft code; from finding the property on the regulating plan all the way through the application and approval process.

other questions?

Thanks!